

7,000 FREE HOMESTEADS

IN 1909

**ALONG THE
LINE OF THE**

GRAND TRUNK PACIFIC RY.



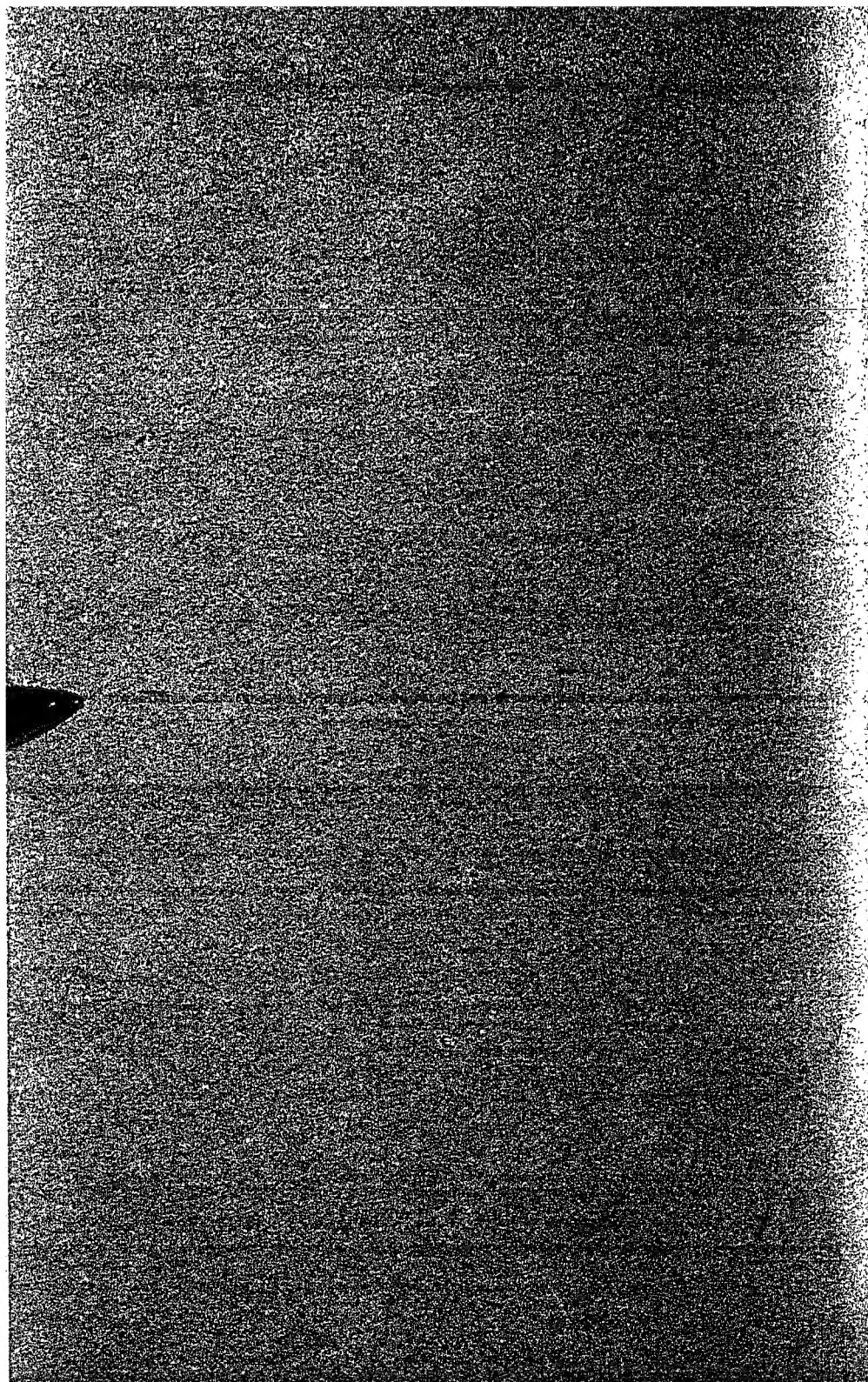
**IN THE MOST
FERTILE SECTION
OF**

WESTERN CANADA

**List of Vacant Lands,
Map and Homestead
Regulations**

PAMPHLET L1. JULY, 1909

**Issued by
General Passenger Department
GRAND TRUNK PACIFIC RAILWAY CO., WINNIPEG, MAN.**



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General Passenger Department
GRAND TRUNK PACIFIC
RAILWAY COMPANY
WINNIPEG, MANITOBA

July 1909

GRAND TRUNK PACIFIC RAILWAY

List of Lands in the Provinces of Saskatchewan and Alberta, Tributary to the Main Line of the Grand Trunk Pacific Railway, open for Homesteading, and a Synopsis of the Canadian Northwest Land Regulations.

THE list is printed for the guidance of intending settlers, and is corrected up to July 1st, 1909.

The list has been carefully compiled, but the Company does not guarantee its complete accuracy. For the latest information respecting any section of land, application should be made to the Agent of Dominion Lands, in whose District the land in question is situated. The address of the Agent will be found on each page of the list.

A list corrected up to latest information available, and particular information regarding any section of the territory desired, will be furnished upon application to the General Passenger Agent, Grand Trunk Pacific Ry., Somerset Block, Winnipeg, Manitoba.

Information will be cheerfully given prospective settlers by any Agent, Ticket Agent or District Official of the Passenger Department of the Grand Trunk Pacific Ry., or Grand Trunk Railway System, with a view to assisting those desirous of settling in Western Canada. Correspondence or personal calls are solicited in this connection. A list of principal agents is shown on page thirteen.

The lands herein listed are all in the provinces of Saskatchewan and Alberta. The section of Manitoba traversed by the Grand Trunk Pacific Ry. is well settled and no free homestead lands are now available along that section of the line.

7000 FREE HOMESTEADS

Along the Grand Trunk Pacific Railway

The Canadian Government has probably never given effect to any Act, which has proved wiser or more important in the building up of Western Canada than that which enables a man who is willing to work, but may be without capital, to acquire a comfortable, healthy home and good farm practically without any expense.

Through this means as a start the tenant, farmer's son or farm laborer may, without much effort, become independent and even wealthy. Much, if not all, of the land available for free homesteading convenient to the lines of existing railways has been taken up. Through the building of the Grand Trunk Pacific millions of acres of land available for homesteads have been rendered accessible, and are now being taken up at such a rate as to make it highly probable that they will be all gone before many months have elapsed, as the Grand Trunk Pacific has been located and built with the object of penetrating the most fertile sections of Western Canada. With few exceptions, all of the lands referred to in the list contained in this publication are among the choicest to be found in the entire West, with the advantage too, that a very large proportion of the older lands in Saskatchewan and Alberta are suitable only for grain growing, while the country along the line of the Grand Trunk Pacific contains much land as well that is ideal for mixed farming, with none of the drawbacks of other districts, such as lack of fuel, timber and water.

There are no barren districts in the territory traversed by the Grand Trunk Pacific, although some townships have not been settled on account of remoteness from transportation facilities hitherto, or reports that because some parts had been found light that the entire townships must be similar. Experience shows that splendid sections have been located in such townships, and when land is well taken up in the locality, the balance of the sections are found to be good enough to raise crops regularly which will average up every year with seemingly more favored sections.

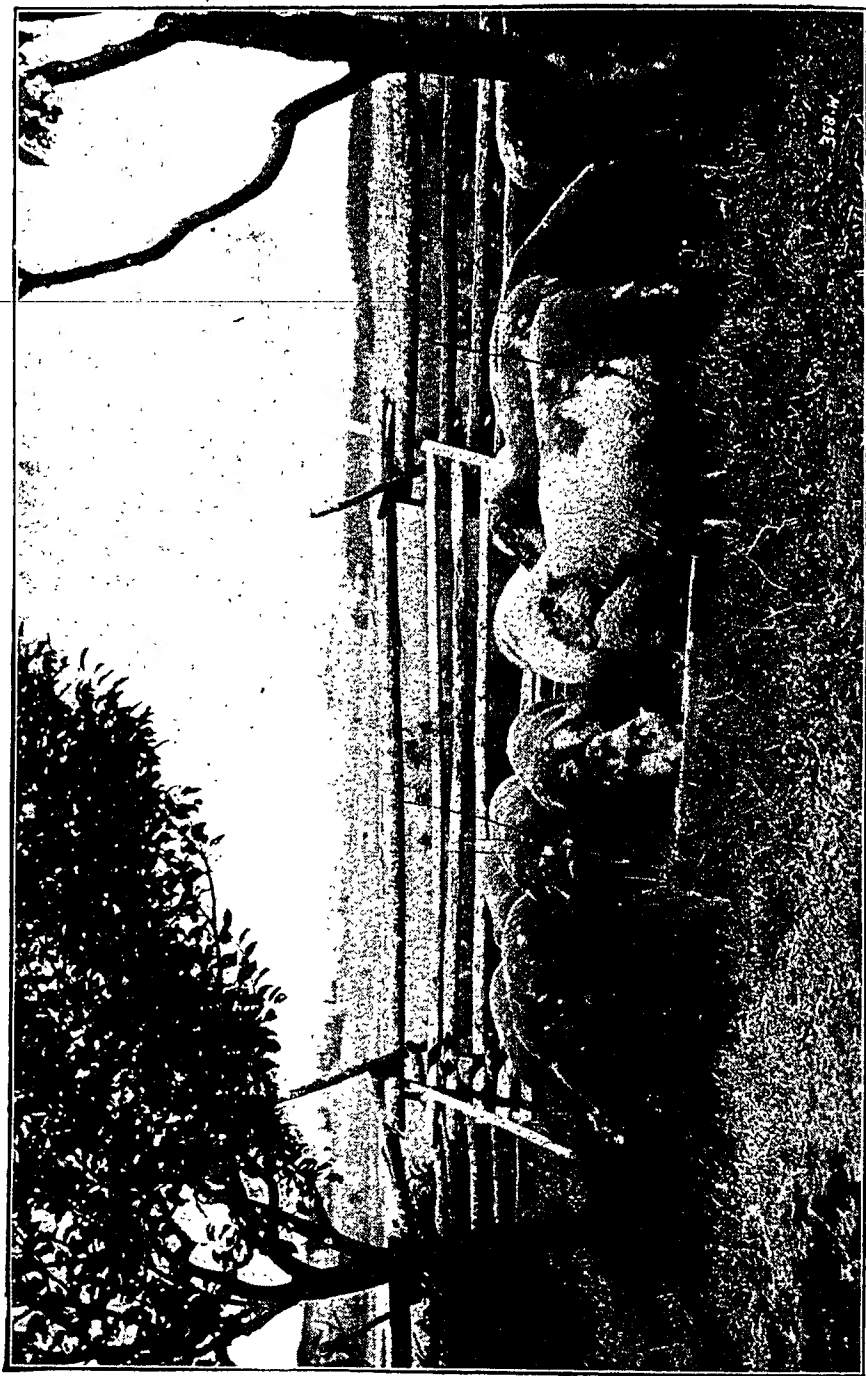
It would be well, therefore, not to overlook the possibilities in the townships in which virtually all the land is shown to be still open for homesteading, as by careful examination many of these townships will be found to contain splendid sections, which will be all the more valuable for being close to the Grand Trunk Pacific.

It is apparent that a great advantage will be gained in being located along the Grand Trunk Pacific, the only All-Canadian Trans-Continental route, which will guarantee that quick settlement of the lands will take place, bringing with it all the social and educational advantages, which are so desirable in a new country.

INTEREST OF THE DOMINION AND PROVINCIAL GOVERNMENTS IN THE SETTLER

The greatest possible assistance is given the settler in all branches of agriculture, stock and poultry raising, dairying and fruit culture by the Dominion and Provincial Governments, whose experimental farms and stations are located at different points in the Western Provinces, from which bulletins are issued regularly (free for the asking), seed grain and grasses distributed at low cost, and practical farmers sent out periodically to lecture and demonstrate in the new communities; a guarantee that the settler will have more advantages in Western Canada at the outset than elsewhere to be found in any country.

The large packing plants near Edmonton and at Winnipeg on the line of the Grand Trunk Pacific furnish a guarantee that cattle, sheep and hogs will find a ready



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market at good prices. Poultry raising, too, will be found highly productive, as the markets of Winnipeg, Melville, Saskatoon, Edmonton and many rapidly growing centres now rely on the East for their supply to a large extent, and prices are necessarily very high. A similar condition exists with respect to dairying, in which branch highly profitable results await the new comer. Alberta now ships some of the product of her creameries to the Yukon and the Orient, besides supplying the local demand. The industry is also making rapid strides in Manitoba, and some considerable advances recently in Saskatchewan.

Throughout the route from Winnipeg west there is no unsightly or arid country, which is noticed over extensive sections of other prairie routes. The sloughs or lakes are fresh or slightly alkaline as a general rule, and over a large area the land is particularly suitable for mixed farming, the natural grasses also being abundant and nutritious for stock.

NECESSITIES OF THE HOMESTEAD

A sufficient supply of good water for domestic purposes is obtainable throughout the entire territory offered to the homesteader along the Grand Trunk Pacific although in some cases the wells must be from 25 to 75 feet, but this condition is not objectionable in prairie sections, as is well known.

Fuel coal, which is described as Lignite, because it is not strictly bituminous coal, but is much superior to that known as lignite in other sections, is mined at present along the line of the Grand Trunk Pacific near Edmonton and Tofield, although the country near Wainwright shows coal deposits and again around Touchwood. This ensures a reliable supply of excellent coal at comparatively low prices, if the wood in any locality is insufficient for any domestic demand. Wood is more plentiful along the Grand Trunk Pacific than in other localities of the prairie section of Western Canada, and is scarce over a small portion of the country only; but eliminate the territory from Raymore to Undora, Xena to Duro and Biggar to Vera, and there is generally sufficient wood for fuel and small buildings.

BUILDING MATERIAL

With the opening of the Grand Trunk Pacific in 1909 to the McLeod River in Alberta, an excellent timber country is reached, assuring settlers of a plentiful and cheap supply of building timber and lumber. Until that supply is available, it will be delivered from Edmonton, Prince Albert and other lumber manufacturing points, so that a supply of the domestic materials necessary in the creation of a prosperous farm will be available at any point on the Grand Trunk Pacific, and at moderate prices. As a rule, all the townsites so far established along the line have from one to five lumber yards, ensuring a regular supply at competitive prices.

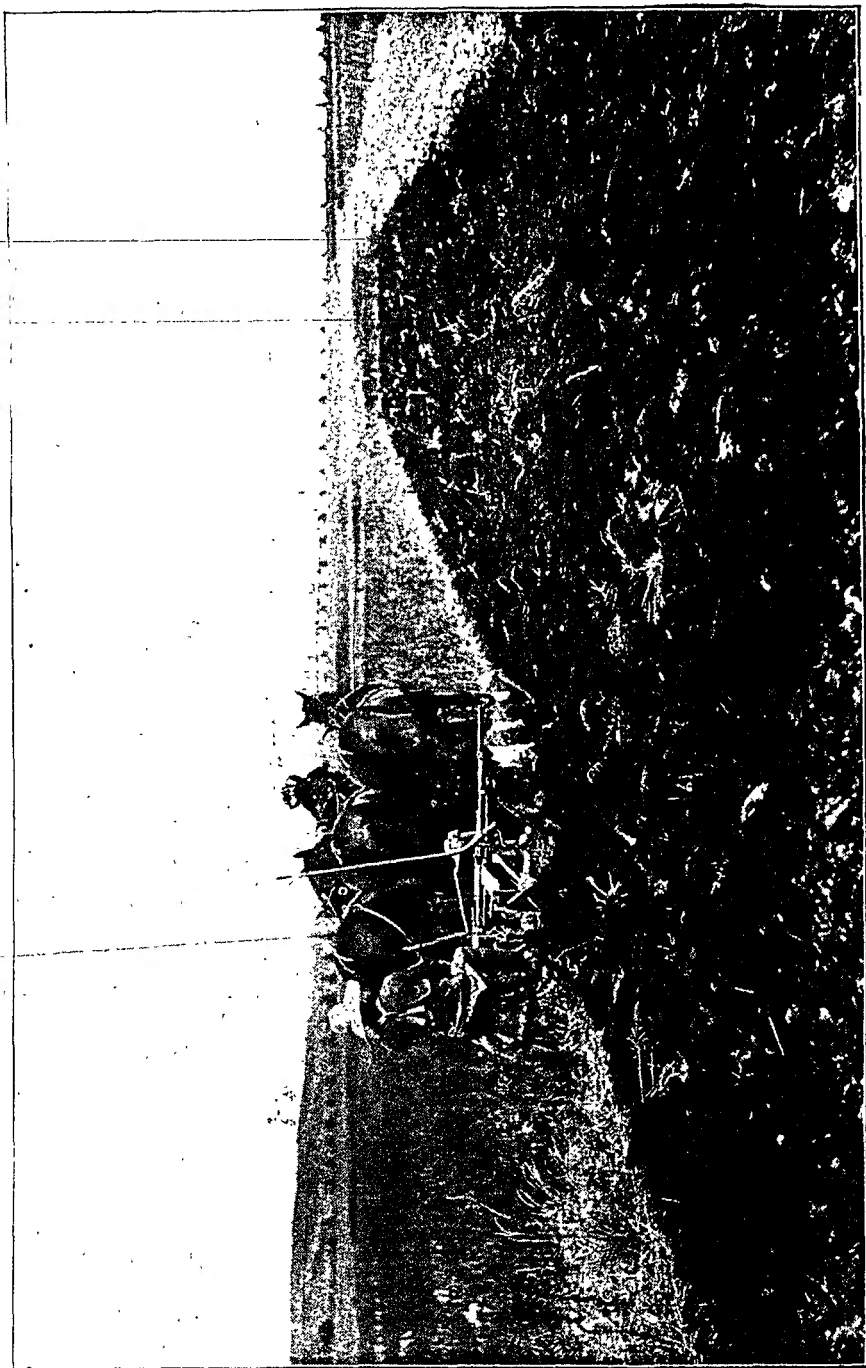
MARKETS

The Grand Trunk Pacific Railway, besides being the only all-Canadian trans-continental route from the Atlantic to the Pacific Ocean, is the first of the trans-continental lines to be constructed in thoroughly modern fashion, so that settlers are assured the lowest cost and most reliable means of transportation in reaching eastern markets, or the ocean ports on the Atlantic or Pacific and on Hudson Bay. It is anticipated that a large part, if not all, of the grain for export to European markets from Saskatchewan and Alberta in the near future will find a cheap route via Prince Rupert over the Grand Trunk Pacific, thence by steamship through the Panama Canal. The railway is being built with this belief in view, and in consequence of the low grades through the mountains, the cost of haulage will probably enable the business to be profitably handled at rates to compare favorably with those applying over the prairie section of the line, which is not nearly possible over any other route.

For the products which move eastward the Grand Trunk Pacific is constructing adequate storage facilities at Fort William on Lake Superior, where by water transportation every export market available for any grain grown in the most favored localities in North America is furnished the farmer along the Grand Trunk Pacific in Western Canada, besides opening the local markets in Eastern Canada on the Grand Trunk Railway System, which is the pioneer railway, and best serves with its connections every section of the eastern provinces.

The Trans-Continental Line passing north of Lake Superior to the Atlantic seaboard at Montreal, Quebec, Halifax, and St. John, is being constructed in such a

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GRAND TRUNK PACIFIC RAILWAY

manner that the flow of export grain from Western Canada will be continuous throughout the year, instead of as at present, being stored for the opening of navigation at the Great Lakes, which means that the grain grower along the Grand Trunk Pacific should receive an additional price for his grain to the extent of the storage and carrying over charges, which will be a considerable advantage over that possessed by others.

CLIMATE AND TEMPERATURES

The ancient and long existing fallacies that Western Canada was not habitable, or could not afford sustenance to any but trappers and fur-traders are now so thoroughly exploded that the most ignorant only could retain any such erroneous ideas, so that remarks on climatic conditions are scarcely necessary in 1909. Suffice it to say that although the winter is cold and long, (but not more so than in certain Eastern Canadian cities, where the invigorating winter climate is the glory of the population, and excepting that there is less snow,) the winter climate of Western Canada is similar to that of Minnesota or Dakota. There is no spring, and a long warm summer with several hours more sunshine per day than in more southerly agricultural countries gives more rapid growth and quicker harvest than elsewhere, removing danger from frost, making the crops sure whether light or heavy. Moisture is held in the earth on the one hand by the depth of frost in the clay slowly coming up, and the melting of the snow, the fall of which is light and seldom exceeds a foot during the entire winter, slowly sinking in, as there is little or no surface drainage to run it off; thus ensuring a renewal of the strength of the soil every year, and making it very doubtful if artificial fertilization will be necessary for many years, as it seems impossible with ordinary precautions to exhaust the lands. The rainfall is light but sure, and always sufficient. No violent storms or cyclones have ever been experienced in Western Canada.

CROPS

All cereals except corn are now grown to perfection in Western Canada in the section traversed by the Grand Trunk Pacific Railway. Wheat, oats, barley, flax, potatoes and garden vegetables, thrive in very few areas, as well as in this "Last West." The growing of flax is an almost universal necessity to the new comer in the first year or two in breaking the soil, and working his land into wheat growing shape, ensuring him a crop the first year, which will maintain him until he has enough land made suitable for his sure and rich harvest.

These homestead lands are in Saskatchewan and Alberta, where the crop averages have been in the last ten years about 20 bushels per acre for wheat, as compared with 13 bushels in Minnesota, only 15 in Iowa, 12 in Nebraska, nearly 13 in North Dakota, and a little over 10 in South Dakota.

It is not uncommon to find oats running 80 to 110 bushels to the acre, and weighing 40 lbs. or more to the bushel.

PLANNING TO HOMESTEAD

Homesteading being the method whereby a farm of 160 acres worth from \$1,600 to \$3,200 may be procured for an entry fee of ten dollars, and residence on the land for a part of three years, some capital is required, as the land cannot be worked profitably, or a living secured during residence, unless stock and machinery are obtained. The Canadian Government handbooks advise against a homesteader entering if he has not \$250 or over, or its equivalent.

It is pointed out, however, that there is always a demand for farm laborers throughout the year at good wages, which during harvesting run from \$2.00 to \$2.50 per day, and in addition there is a large amount of railway and other construction work in prospect for several years to come, so that it will not be difficult, or take long, for the worker to get together the necessary capital to make a start on his homestead.

The Canadian Government issues attractive homestead literature, which deals very fully with the requirements of the prospective settler with which every one with an inclination to take up land in Western Canada should familiarize himself. Procure a copy of "The Last Best West" from the nearest Canadian Government Immigration Agent, or from the General Passenger Agent, Grand Trunk Pacific Ry., Somerset Block, Winnipeg.

DO NOT DELAY

unnecessarily in taking up one of the 7,000 homesteads listed in this pamphlet, as they will not be open very long, especially those nearest the railway



STABLES AND HOUSES OF A SUCCESSFUL ALBERTA FARMER

GRAND TRUNK PACIFIC RAILWAY

TO THE YOUNG MAN

this opportunity should appeal strongly and now,

TO THE RENTER

this opportunity to stop paying rent should be irresistible, but

TO THE FATHER

with growing sons this opportunity to provide each with a farm around his own should not be neglected. In a short time regrets will be vain, as the "Best West" is also the "Last West," and free lands come again no more.

INFORMATION

If any further information is required, or anything in this pamphlet not understood, call on or write any agent shewn herein, or write direct to the General Passenger Agent, Grand Trunk Pacific Railway, Somerset Block, Winnipeg, Manitoba.

In Western Canada taxes are very low. Schools are endowed and may be established in any section where there are six or more children.

Lands are largely rolling prairie with one to two feet black loam on a clay subsoil.

Grazing leases are issued to settlers, if pasturage required.

Hay leases are issued to settlers if required in addition to that cut on their own quarter sections.

Free timber permits are issued to settlers for building material if no suitable timber on their own property.

Markets are available owing to excellent transportation facilities.

The climate is hot in summer, and cold in winter—the best conditions for grain growing; besides there is much more daylight and sunshine in the growing season, which means no summer frosts.

Fuel is easily and reasonably procured.

Stock raising and mixed farming find ideal conditions along the Grand Trunk Pacific Railway.

The Grand Trunk Pacific offers the assistance of its Agents, and the Grand Trunk Railway System likewise, to prospective settlers and homesteaders.

Call and you will be cheerfully received.

SYNOPSIS OF CANADIAN NORTHWEST LAND REGULATIONS

1. Any person who is the sole head of a family, or any male over 18 years old, may homestead a quarter-section (160 acres, more or less) of available Dominion land in Manitoba, Saskatchewan or Alberta. The applicant, who must be a British subject or declare his intention of becoming one, must appear in person at the Dominion Lands Agency or Sub-Agency for the district. Entry by proxy may be made at any agency, by father, mother, son, daughter, brother or sister of intending homesteader, when duly authorized on proper form.

2. A widow having minor children of her own dependent upon her for support is permitted to make homestead entry as the sole head of a family.

DUTIES.—Six months' residence upon and cultivation of the land in each of three years. A homesteader may live within nine miles of his homestead on a farm of at least 80 acres solely owned and occupied by him or by his father, mother, son, daughter, brother or sister.

3. In certain districts a homesteader in good standing may pre-empt a quarter-section alongside his homestead. Price \$3.00 per acre. Duties.—Must reside six months in each of six years from date of homestead entry (including the time required to earn homestead patent) and cultivate fifty acres more than required on his homestead, which cultivation may be on both his homestead and pre-emption, or either.

4. A homesteader who has exhausted his homestead right by already homesteading and cannot obtain a pre-emption may acquire a homestead by purchase in certain districts. Price \$3.00 per acre. Such homesteads may be acquired on any

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available lands on either odd or even numbered Sections south of township 45, east of the railway from Calgary to Edmonton and the west line of range 26, and west of the third Meridian. Duties.—Must reside six months in each of three years, cultivate fifty acres and erect a house worth \$300.

The entry fee for a homestead is ten (\$10) dollars.

NOTE:—The townships in the districts mentioned in the third and fourth paragraph of these regulations have been noted in this list.

VOLUNTEER LAND GRANT WARRANT

A grantee or substitute being a British subject may homestead two adjoining quarter-sections. If entry is not applied for on or before 31st December, 1910, the right will lapse. Settlement duties same as those to be performed by ordinary homesteader.

A grantee may also obtain entry the same as an ordinary homesteader, but his residence on the homestead cannot be counted in connection with the land grant, nor can the grantee live on the land grant and do residence thereon for a homestead in the vicinity until he has earned title to the land grant. This, however, does not debar him from putting a six months' residence in each year on the homestead itself and in accordance with the regulations, if he is in a position to do so.

SETTLERS' FREIGHT RATES

Low rates for settlers' effects apply from Eastern Canada and many United States points to Winnipeg and West.

The following is a summary of the Customs and Freight regulations:—

CUSTOMS REGULATIONS

The following is an extract from the customs tariff of Canada, specifying the articles that can have free entry:

Settlers' Effects, viz.:—Wearing apparel, household furniture, books, implements and tools of trade, occupation, or employment; guns, musical instruments, domestic sewing machines, typewriters, live stock, bicycles, carts, and other vehicles, and agricultural implements in use by the settler for at least six months before his removal to Canada, not to include machinery or articles imported for use in any manufacturing establishment or for sale; also books, pictures, family plate or furniture, personal effects, and heirlooms left by bequest; provided, that any dutiable articles entered as settlers' effects may not be so entered unless brought with the settler on his first arrival, and shall not be sold or otherwise disposed of without payment of duty until after twelve months' actual use in Canada.

Settlers arriving from the United States are allowed to enter duty free stock in the following proportions: One animal of neat stock or horse for each ten acres of land purchased or otherwise secured under homestead entry, up to 160 acres, and one sheep for each acre so secured. Customs duties paid on animals brought in excess of this proportion will be refunded for the number applicable to an additional holding of 160 acres, when taken up.

The settler will be required to fill up a form (which will be supplied him by the customs officer on application) giving description, value, etc., of the goods and articles he wishes to be allowed to bring in free of duty. He will also be required to take the following oath:

I,, do hereby solemnly make oath and say that all the goods and articles hereinbefore mentioned are to the best of my knowledge and belief entitled to free entry as settlers' effects under the tariff of duties of customs now in force, and that all of them have been owned by myself for at least six months before removal to Canada; and that none of the goods or articles shown in this entry have been imported as merchandise for any use in a manufacturing establishment or as a contractor's outfit, or for sale, and that I intend becoming a permanent settler within the Dominion of Canada, and that the "Live Stock" enumerated in the entry hereunto attached, is intended for my own use on the farm which I am about to occupy (or cultivate), and not for sale or speculative purposes, nor for the use of any other person or persons.

Sworn before me. this. day of. 190

Collector.

GRAND TRUNK PACIFIC RAILWAY

FREIGHT REGULATIONS

1. Carloads of Settlers' Effects, within the meaning of the settlers' tariff, may be made up of the following described property for the benefit of actual settlers, viz.: Live stock, any number up to but not exceeding ten (10) head, all told, viz.: Cattle, calves, sheep, hogs, mules, or horses; Household goods and personal property (second-hand); Wagons or other vehicles for personal use (second-hand); Farm Machinery, Implements, and Tools (all second-hand); Soft-wood Lumber (Pine, Hemlock, or Spruce—only) and Shingles, which must not exceed 2,000 feet in all, or the equivalent thereof; or in lieu of; not in addition to, the lumber and shingles, a Portable House may be shipped; Seed Grain, small quantity of trees or shrubbery; small lot live poultry or pet animals; and sufficient feed for the live stock while on the journey. Settlers' Effects rates, however, will not apply on shipments of second-hand Wagons, Buggies, Farm Machinery, Implements, or Tools, unless accompanied by Household Goods.

2. Should the allotted number of live stock be exceeded, the additional animals will be charged for at proportionate rates over and above the carload rate for the Settlers' Effects, but the total charge for any one such car will not exceed the regular rate for a straight carload of Live Stock.

3. Passes.—One man will be passed free in charge of live stock when forming part of carloads, to feed, water, and care for them in transit. Agents will use the usual form of Live Stock Contract.

4. Less than carloads will be understood to mean only Household Goods (second-hand), wagons or other vehicles for personal use (second-hand), and (second-hand) Farm Machinery, Implements, and Tools. Less than carload lots must be plainly addressed. Minimum charge on any shipment will be 100 pounds at regular first-class rate.

5. Merchandise, such as groceries, provisions, hardware, etc., also implements, machinery, vehicles, etc., if new, will not be regarded as Settlers' Effects, and, if shipped, will be charged at the regular classified tariff rates. Agents, both at loading and delivering stations, therefore, give attention to the prevention of the loading of the contraband articles and see that the actual weights are way-billed when carloads exceed 24,000 lbs. on lines north of St. Paul.

6. Top Loads.—Agents do not permit, under any circumstances, any article to be loaded on the top of box or stock cars; such manner of loading is dangerous and absolutely forbidden.

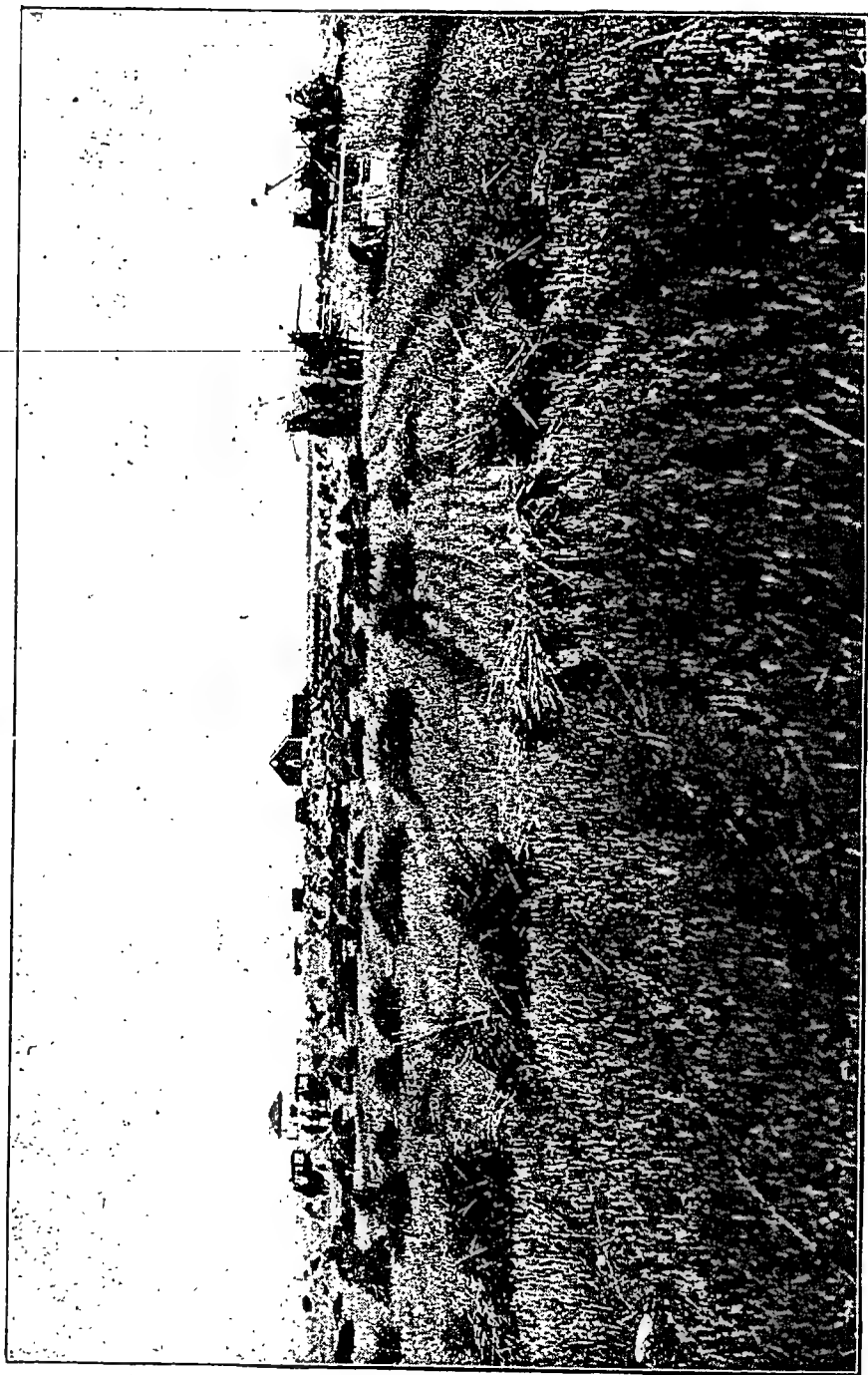
7. Settlers' Effects, to be entitled to the carload rates, cannot be stopped at any point short of destination for the purpose of unloading part. The entire carload must go through to the station to which originally consigned.

8. The carload rates on Settlers' Effects apply on any shipment occupying a car weighing 24,000 lbs. or less. If the carload weighs over 24,000 lbs. the additional weight will be charged for. North of St. Paul, Minn., 24,000 lbs. constitute a carload, between Chicago and St. Paul and Kansas City or Omaha and St. Paul a carload is 20,000 lbs. From Chicago and Kansas City north to St. Paul any amount over this will be charged extra. From points in Eastern Canada via Chicago, 24,000 lbs. is the minimum carload weight. From points South and East of Chicago in the United States only five horses or heads of live stock are allowed in carloads, any over this will be charged extra; carload 12,000 lbs. minimum.

9. Minimum charge on any shipment will be 100 lbs. at first-class rate.

QUARANTINE OF SETTLERS' CATTLE

Settlers' cattle, when accompanied by certificates of health, to be admitted without detention; when not so accompanied, they must be inspected. Inspectors may subject any cattle showing symptoms of tuberculosis to the tuberculin test before allowing them to enter. Any cattle found tuberculous to be returned to the United States or killed without indemnity. Sheep for breeding and feeding purposes may be admitted subject to inspection at port of entry, and must be accompanied by a certificate, signed by a Government inspector, that sheep scab has not existed in the district in which they have been fed for six months preceding the date of importation. If disease is discovered to exist in them, they may be returned or slaughtered. Swine may be admitted, when forming part of settlers' effects, but only after a quarantine of thirty days, and when accompanied by a certificate that swine plague or



HARVESTING OATS IN SASKATCHEWAN

GRAND TRUNK PACIFIC RAILWAY

hog cholera has not existed in the district whence they came for six months preceding the date of shipment when not accompanied by such certificate, they must be subject to inspection at port of entry. If diseased, to be slaughtered, without compensation.

DOMINION LAND OFFICES

J. BRUCE WALKER, Commissioner of Immigration, Winnipeg, Man.

DISTRICT	NAME OF AGENT	POST OFFICE ADDRESS
Battleford.....	L. P. O. Noel.....	Battleford, Saskatchewan.
Edmonton.....	A. Norquay (acting).....	Edmonton, Alberta.
Red Deer.....	W. H. Cottingham.....	Red Deer, Alta.
Humboldt.....	G. L. Dempster.....	Humboldt, Saskatchewan.
Saskatoon.....	D. L. Bettschen.....	Saskatoon, Sask.
Wetaskiwin.....	Business transacted at Edmonton.	
Yorkton.....	P. E. Peaker.....	Yorkton, Saskatchewan.

Land settlers' certificates entitling bona fide settlers to special fares on the Grand Trunk Pacific Railway will be issued by the following:—

CANADIAN GOVERNMENT IMMIGRATION AGENTS IN THE UNITED STATES

M. V. McInnes, 176, Jefferson Avenue, Detroit, Michigan.
 James Grieve, Auditorium Building, Spokane, Washington.
 J. S. Crawford, 125 W. Ninth Street, Kansas City, Missouri.
 E. T. Holmes, 315 Jackson Street, St. Paul, Minn.
 T. O. Currie, 180 3rd Street, Milwaukee, Wis.
 C. J. Broughton, 4th floor, Merchants Loan and Trust Building, Chicago, Ill
 W. V. Bennett, 215 Board of Trade Building, Omaha, Nebraska.
 J. M. MacLachlan, Box 626, Watertown, South Dakota
 C. Pilling, Clifford Block, Grand Forks, North Dakota.
 W. H. Rogers, 3rd Floor, T. T. Building, Indianapolis, Indiana.
 H. M. Williams, Gardner Block, Toledo, Ohio.
 C. A. Laurier, Marquette, Michigan.
 Benj. Davies, Dunn Block, Great Falls, Montana.
 George A. Hall, House Building, Pittsburgh, Pa.
 Thos. Hetherington, 2nd Floor Tremont Building, Tremont Street, Boston, Mass.
 Thos. Duncan, Syracuse Bank Building, Syracuse, N.Y.

LIST OF PRINCIPAL GRAND TRUNK RAILWAY SYSTEM AGENTS

Baltimore, Md.....	Theo. H. Diener & Co.....	Ticket Agents, 217 E. Baltimore Street
Battle Creek, Mich.....	L. J. Bush.....	Passenger Agent, G.T. Station
Bay City, Mich.....	H. G. Smith.....	Passenger Agent, G.T. Station
Boston, Mass.....	E. H. Boynton.....	New England Passenger Agent, 360 Washington St.
Buffalo, N.Y.....	H. M. Morgan.....	City Pass. and Tkt. Agt., 285 Main St. (Ellicott Sq. Building)
Chicago, Ill.....	J. H. Burgis.....	City Pass. and Tkt. Agt., 249 Clark St., cor. Jackson B'v'd.
Cortland, N.Y.....	D. P. Drewery.....	Travelling Passenger Agent, 6 Burgess Block
Detroit, Mich.....	Geo. W. Watson.....	City Passenger and Ticket Agent, 124 Woodward Avenue
Flint, Mich.....	H. H. Earle.....	Passenger Agent, G.T.R. Station
Grand Rapids, Mich.....	C. A. Justin.....	City Passenger and Ticket Agent, G.T.R. Station
Hamilton, Ont.....	C. E. Morgan.....	City Passenger and Ticket Agent, 11 James Street North
Kansas City, Mo.....	P. H. Vair.....	Travelling Passenger Agent, 327 Sheldley Building
Kingston, Ont.....	J. P. Hanley.....	City Passenger and Ticket Agent
Lansing, Mich.....	F. H. Potter.....	Passenger Agent, G.T. Station
Lewiston, Me.....	F. P. Chandler.....	Passenger Agent, G.T.R. Station
London, Ont.....	E. de la Hooke.....	City Pass. and Ticket Agent, cor. Richmond and Dundas Sts.
Los Angeles, Cal.....	W. H. Bullen.....	Pacific Coast Agent, 302 Wilcox Building
Milwaukee, Wis.....	Crosby Trans. Co.....	396 East Water Street
Minneapolis, Minn.....	W. J. Gilkerson.....	T.P.A., 713 Metropolitan Bldg. (formerly Guaranty Bldg.)
Montreal, Que.....	J. Quinlan.....	District Passenger Agent, Bonaventure Station
Mt. Clemens, Mich.....	Caspar Cizek.....	City Passenger and Ticket Agent, 12 South Gratiot Avenue
New York, N.Y.....	F. P. Dwyer.....	General Agent Pass. Dept., Railway Exchange, 290 Broadway
Niagara Falls, N.Y.....	W. J. Hamilton.....	Ticket Agent, 29 Falls Street
Ogdensburg, N.Y.....	Geo. S. Meagher.....	Ticket Agent, 55 State Street
Ottawa, Ont.....	Percy M. Buttler.....	City Pass. & Tkt. Agt., Russell House, Blk. cor. Sparks and Elgin Sts.
Peterboro, Ont.....	W. Buntin.....	City Passenger and Ticket Agent
Pittsburg, Pa.....	W. Robinson.....	Travelling Passenger Agent, 506 Park Building
Pontiac, Mich.....	C. E. Haskell.....	Passenger Agent, G.T. Railway Station
Port Huron, Mich.....	C. R. Clarke.....	Ticket Agent, G.T.R. Station
Portland, Me.....	C. E. Tenny.....	Passenger Agent, G.T. Railway Station
Quebec, Que.....	Geo. H. Stott.....	C.P. & T.A., cor. St. Anne & Du Fort Sts. & Ferry Ldg., Dalhousie St.
Saginaw, Mich.....	Hugh E. Quick.....	Passenger Agent, G.T. Station
San Francisco, Cal.....	F. H. Lord.....	Agent, 399 Monandnock Building
Sherbrooke, Que.....	C. H. Foss.....	City Passenger and Ticket Agent, 2 Wellington Street
South Bend, Ind.....	C. A. McNutt.....	Passenger Agent, G.T. Station
Toronto, Ont.....	J. D. McDonald.....	District Passenger Agent, Union Station
Winnipeg, Man.....	A. E. Duff.....	General Agent Passenger Department, 260 Portage Avenue

ANNOUNCEMENT

IN response to many inquiries on the subject which have been received by the Company from all parts of the world, a booklet has been issued by the Grand Trunk Pacific Railway Company for the purpose of giving information respecting its western terminus on the Pacific Ocean at

PRINCE RUPERT BRITISH COLUMBIA

the new city now being built on the northern British Columbia Coast.

There has been acquired in the interest of the Railway Company twenty-four thousand acres of land at Prince Rupert and vicinity for the purpose of the townsite and the development of the Port, one quarter interest in which belongs to the *Province of British Columbia*, who are therefore jointly interested with the Railway Company in the development of this *New Seaport*.

The first subdivision of the townsite covers an area of two thousand acres. For particulars of sale of lots apply to G. A. RYLEY, Land Commissioner, Winnipeg, Man.

Copies of this booklet may be obtained upon application to the Secretary of the Grand Trunk Pacific Railway Company; Montreal, the Land Commissioner at Winnipeg, Manitoba, Canada, or the General Passenger Agent at Winnipeg, Manitoba.

LIST OF

CANADIAN GOVERNMENT LANDS available for FREE HOMESTEADS

ALONG THE LINE OF THE

GRAND TRUNK PACIFIC RAILWAY

SECTION	T'ahp.	R'go.	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
N.W. 16, 20, 32, 36, S.W. 14, S. 36 } N.E. 22, 26, W. 22, S.E. 20, 32 } 24, 28 }	18	30	1	(c) Welby, Sask., Sec. 0...	Yorkton, Sask.
N.E. 13, N. 2, S.E. 1, S.E. 13...	19	30	1	(c) Welby or (c) Spy Hill, Sec. 2, 19, 31...	do
N.W. 2, S.W. 4, 22...	18	31	1	do	do
N.W. 12, E. 21*	21	33	1	(c) Zeneta, Sec. 22, 20, 1...	do
N.W. 14...	20	1	2	do	do
N.E. 10, S.E. 22...	22	2	2	(c) Atwater, Sec. 4, 21, 2 or (c) Bangor, Sec. 15, 21, 3	do
30...	23	3	2	(c) Waldron Sec. 33, 21, 4	do
N.E. 4...	21	4	2	do	do
S.W. 4...	22	4	2	do	do
N.W. 14, 18...	22	6	2	(c) Melville, Sec. 20, 22, 6	do
N.W. 23, N.E. 23, S.W. 15, S. 23...	25	6	2	do	do
N.W. 20...	23	7	2	Birmingham, Sec. 16, 23, 7	do
N.W. 14...	22	8	2	do	do
S.E. 32...	23	8	2	(c) Fenwood, Sec. 27, 23, 8	do
N.E. 20, S.E. 20...	24	8	2	(c) Goodeve, Sec. 10, 24, 9	do
N.W. 16, 18, N.E. 32, S.W. 4... } S.E. 32, W. 30... }	25	8	2	do	do
S.E. 6...	26	8	2	do	do
N.W. 32, S.W. 18, 24, N.E. 12, 22 } N. 14, N. 24, S.E. 14, 16, 24 } 34 }	25	9	2	do or (c) Hubbard, Sec. 5, 25, 10	do
N.W. 12, 14, 16, 22, 28... 30... } S.W. 14, 16, 28... 32 } N.E. 12, 18, 22, 24, S.E. 22, 28... } S. 18... 34 }	27	9	2	do	do
S.W. 16, N.E. 14, 16, N. 4... } S. 4... 10, S. 14... }	28	10	2	(c) Kelliher, Sec. 34, 26, 13	do
N.E. 24, S.E. 22...	24	11	2	(c) Hubbard...	do
W. 20*	26	11	2	Ituna, Sec. 21, 25, 11	do
N.W. 36, N.E. 24... E. 13... } S.W. 25, S.E. 1, 28, 36... }	27	11	2	(c) Kelliher...	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
N.W. 7, N.E. 3, 12, 15, 23, S.W. 1, 19 } S.E. 5, 16, 22, N...1, N...5 N...19, S...14, S...15, S.W. 17...2, ...W. 6, N.E. 13, N.E. 21W 27, ...S.W. 31...S.W. 13.... }	28	11	2	(c) Kelliher or (c) Jasmin, Sec. 9, 26, 12.....	Yorkton, Sask,
S.W. 14.....	24	12	2	Ituna.....	do
S.W. 28, N.E. 36.....	25	12	2	(c) Jasmin.....	do
N.W. 18, S.W. 36, N...36.....	27	12	2	(c) Kelliher.....	do
N.E. 20, N.W. 33, S.W. 33, S.E. 25...	28	12	2	do.....	do
N.E. 2*.....	26	13	2	do.....	Humboldt, Sask,
N.W. 33, S.W. 24, 34..... } N.E. 33, 34, N...31, N...35..... S...33..... }	27	13	2	(c) Kelliher, or (c) Leross, Sec. 11, 27a, 14...	do
N.W. 6, 35, S.W. 1, 13, 15, 33,2 } N.E. 10, 13,17.....19,21, S.E. 14, 16, 23...7,9,18, N...1... N...3, N...33, S...10, ...W. 16...27, N...15..... }	28	13	2	(c) Leross.....	do
N.W. 10, 4, S.W. 4.....	20	13	2	do.....	do
S.W. 32, N.E. 26, 32..... } S.E. 28, N...36, ...E. 14.....22,34..... }	27	14	2	do.....	do
N.W. 14, S.E. 4, 14, S.W. 12, 14..... }15, N...12,2,3 }	28	14	2	do.....	do
S.E. 2.....	29	14	2	do.....	do
.....6.....	27A	15	2	Mostyn.....	do
N.W. 20.....	27	15	2	(c) Touchwood, Sec. 23, 27, 16	do
S.W. 12, N.E. 4, 26.....	28	15	2	do.....	do
N.W. 18, 31, N.E. 19, ...W. 30....	25	16	2	do.....	do
N.E. 3, 10, 26, 36, S.E. 7, 13, 24... }25, N...7, N...13, N...22...23, N...24, S...36,35 15, *27*, 34*..... }	26	16	2	do.....	do
.....1,2,12, E. 3*....	27A	16	2	do.....	do
S.W. 22, 24, 32, N.E. 18, 26, 27, 30... } S.E. 2 ...E. 22, N...24..... }	28	16	2	do.....	do
N.E. 6,34,32, 28*, 30*..	29	16	2	(c) Touchwood or (c) Punnichy, Sec. 11, 27, 17	do
S.W. 12, 16, 32, N.E. 10, ...E. 36....	25	17	2	(c) Quinton, Sec. 12, 28, 18	do
S.W. 7, N.W. 17*, N...12, W. 20*, 32*	26	17	2	(c) Punnichy.....	do
N.W. 18, S.W. 24*, S.E. 6, S...36...	27	17	2	do.....	do
N.W. 31, N.E. 14, S.E. 32, S...21... } W. 12*..... }	28	17	2	do.....	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
N.W. 10, S.W. 2, S.W. 12*, N.E. 2, 9 S.E. 9*, S.E. 3, 15, E. 10. E. 16, E. 34, 14, 22, 27, W 35, W. 1* 23*, W. 13*, 24*, E. 21*, 28*....	29	17	2	(c) Punnichy.....	Humboldt, Sask
S.E. 30.....	25	18	2	(c) Quinton.....	do
N.W. 6, S.W. 34, S.E. 2, 24.....	26	18	2	do	do
S.W. 6, 32.....	29	18	2	(c) Raymore, Sec. 30, 28, 18.	do
S.E. 34.....	26	19	2	do	do
N.W. 20.....	27	19	2	do	do
N.W. 2, N.E. 30, S.E. 34.....	28	19	2	(c) Raymore or (c) Semans, Sec. 23, 28, 20.....	do
S.E. 4.....	29	19	2	do	do
S.W. 32, 30, 31..... N.E. 32, S.E. 27*.....	30	19	2	do	do
N.E. 26, N. 32, 33.....	29	20	2	(c) Semans or (c) Tate, Sec. 36, 28, 21.....	do
S.W. 2, 18, N. 12, 24..... N.E. 5, 17, 18, 32, 4, 16 13, S.E. 19, 30..... 36, 33, 34, W. 3 10, 9, 14, 25 35, 15*, 20*, 21*, 22*, 23* 27*, 28*, S. 32.....	30	20	2	(c) Tate.....	do
..... W. 31.....	28	21	2	(c) Tate.....	Regina, Sask.
N.E. 26.....	29	21	2	do	do
S.W. 1, 12, S.E. 2, 4.....	30	21	2	(c) Tate or (c) Nokomis, Sec. 27, 29, 22.....	Humboldt, Sask
N.W. 30, S.W. 4, 9, N.E. 4, 7..... S.E. 6, W. 6.....	29	22	2	(c) Nokomis.....	Regina, Sask.
N.W. 27, N. 36..... S.W. 21, 24, 25, 32, N.E. 25, 28..... S.E. 28, 36, S. 17, S. 27, E. 31 N. 21*, 18*, N. 33..... S.E. 33..	28	23	2	do	do
N.W. 10, S.W. 22, N.E. 30, 34..... S.E. 1, 34.....	29	23	2	(c) Nokomis or (c) Undora Sec. 15, 30, 23.....	do
N.E. 18, S.E. 6, W. 8, W. 20. W. 19*.....	30	23	2	(c) Undora.....	Humboldt, Sask.
..... 6, S. 18.....	31	23	2	(c) Undora or (c) Venn, Sec. 34, 30, 24.....	do
N.W. 7, S.W. 6, S.E. 20, S.W. 7..... S.E. 14, 32.....	32	23	2	(c) Venn.....	do
.....	28	24	2	(c) Undora.....	Regina, Sask.
N.W. 14, 24, 36..... S.W. 14, 36, N.E. 14.....	29	24	2	(c) Undora or (c) Venn.....	do
S.W. 24*, N.E. 36*, S.E. 12, E. 25*..	30	24	2	(c) Venn.....	Humboldt, Sask.
N.W. 16, N.E. 26, S. 12, N. 1.. N. 15, N. 22, E. 14, E. 32 13, 28, 33.....	31	24	2	(c) Venn or (c) Watrous, Sec. 22, 31, 25.....	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
S.W. 13, 24, N.E. 13*, S.E. 14, E. 6*. ...W. 18, ...W. 19, 2*, ...N. 7	32	24	2	(c) Watrous.....	Humboldt, Sask.
S.E. 36	28	25	2	do	Regina, Sask.
N.W. 12	29	25	2	do	do
	30	25	2	do	Humboldt, Sask.
N.W. 16, S.W. 22, 30, S.W. 9*..... N.E. 6, S.E. 27, 23, N. 34..... S. 16, S. 19, ...W. 15, 17*, 18* N. 9*, N. 10*.....	32	25	2	(c) Watrous.....	do
N.W. 4, 7, 17, N. 10, S.E. 7, 10, 17 S. 16, ...W. 13, ...3, ...W. 15	33	25	2	do	do
N.W. 4, N.W. 12*, S.W. 5, S.W. 13* S.W. 35*.....N.E. 34* S.E. 12*, N. 5, 8, 14, ...W. 1* ...W. 16, ...W. 21, ...W. 23..... ...W. 28, ...W. 31..... ...17, ...18, ...19, ...20 ...30, ...E. 7, N.E. 14*.....	29	26	2	do	Regina, Sask.
N.W. 4, 7, 10, N.W. 3*-21*, S.W. 30, N.E. 17, S.E. 4, 10, ...6, ...N.E. 16, ...20, ...31, N. 30, ...W. 18, ...9*, W. 28*, W. 33* ...S. 16	30	26	2	do	Humboldt, Sask.
N.E. 21, S.E. 4, 14, S. 5.....	31	26	2	Xena, Sec. 3, 32, 26.....	do
N.W. 21, S.W. 16, 35, S.W. 20* N.E. 7, 26, 31, S.E. 18, 24, 32, 34 N. 18, N. 27, N. 32, N. 23* N. 16*, S. 31, ...E. 30, ...19 ...13*, 17*.....	32	26	2	Xena or (c) Young, Sec. 27, 32, 27.....	do
N.W. 1, 30, 32, 35..... S.W. 3, 12, 14, 18, 19, 30, 35..... N.E. 12, 28, S.E. 17, N. 27..... S. 5, ...E. 6, ...W. 7, ...31	33	26	2	(c) Young.....	do
S.W. 18, 36, N.E. 21, 36, S.E. 36..... N. 18, S. 24, ...1, ...2 ...3, ...4, ...5, ...6 ...7, ...9, ...10, ...12 ...13, ...14, ...16, ...17 ...20, ...25, ...30, ...32 ...35.....	30	27	2	(c) Watrous.....	do
N.W. 12, 21, 27, S.W. 4, 6, S.E. 3, S.E. 31*, S.W. 31*, S. 12, S. 13 ...5.....	31	27	2	Xena or (c) Young.....	do
N.W. 16, 20, 24, 36, N.E. 24, 26..... S.E. 24, ...E. 9, ...W. 35, ...13 ...23, ...25, ...N. 33, ...S.E. 33	32	27	2	(c) Young.....	do
S.W. 2, 3, 16, 17, 24, N.E. 3, 4, 15, 25... S.E. 13, N. 1, N. 12, ...5, ...9	33	27	2	(c) Young or Zelma, Sec. 15, 33, 28.....	do

Canadian Government Lands—Continued

SECTION	T'shp.	R't'go	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
Entire, except Secs. 8, 11, 20, 29, ... }	30	28	2	(c) Young, ...	Humboldt, Sask
Entire except Secs. 8, 11, 12, 23, 24, 25, 26, 27, 29, 33, E. 31, W. 34, S. 36, ... }	31	28	2	(c) Young, ...	do
N.W. 4, 15, 21, N.E. 22* ... N.E. 26*, N.E. 31, N. 13, S. 2 ... S. 4, S. 9, 1, 3, 5 S. 6, ... }	32	28	2	(c) Young or Zelma, ...	do
N.W. 13, 25, N.W. 33*, S.W. 24*, 25 S.E. 13, N.E. 26, 32, 35, N.E. 27* ... N. 23*, S. 33*, S. 35, ... E. 36, ... }	33	28	2	Zelma, ...	do
S.W. 9, N.E. 7, 12, S.E. 4, N. 3, ... 1, ... }	34	28	2	Zelma or (c) Allan, Sec. 103, 4, 1 W. 3rd, ...	do
N.W. 25, S.E. 25, E. 3, E. 10 E. 15, E. 22, E. 34, 1 2, 12, 13, 14 23, 24, 35, 36 }	30	29	2	(c) Young, ...	do
N.E. 26, E. 2, E. 14, E. 23 E. 35, 1, 12, 13 24, 25, 36 }	31	29	2	(c) Young, ...	do
S.E. 14, E. 2*, 1, ... }	32	29	2	Zelma, ...	do
S.W. 33, N.E. 26, S.E. 27, N. 33, S. 33, and balance except Secs. 3, 8, 11, 29, 34, ... }	31	1	3	do	Saskatoon, Sask
	See Note A & B				
N.W. 9, S.W. 4, N.E. 12, ... S. 9, S. 12, S. 6, N.E. 15, ... 1, ... }	32	1	3	do	do
	See Note B				
S.W. 12*, S.W. 14, ... }	33	1	3	(c) Allan, ...	do
	See Note B				
N.W. 13, N.E. 24, S.E. 28, ... }	34	1	3	(c) do	do
	See Note B				
N.W. 33*, N.E. 32*, N. 7*, ... }	34	2	3	(c) Bradwell, Sec. 28, 34, 2	do
	See Note B				
N.W. 32*, S.W. 13, S.W. 30*, ... N.E. 13, N. 31*, ... }	35	2	3	(c) Bradwell, ...	do
	See Note B				
S.W. 27, ... }	34	3	3	(c) Bradwell or (c) Clavet, Sec. 9, 35, 3, ...	do
	See Note B				
N.W. 5, 7, S.W. 5, 18, N.E. 6, S.E. 5, 6 N. 18, S. 7, ... }	34	4	3	(c) Clavet, ...	do
	See Notes A and B				

Canadian Government Lands—Continued.

SECTION	T'shp.	R'go	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
13.....	36	4	3	Duro, Sec. 33,35,4.....	Saskatoon, Sask.
	See Notes A and B				
N.W. 1, S.W. 2,4, N.E. 1,4.....	35	6	3	(c) Earl, Sec. 16,36,5 o Farley, Sec. 20,36,6...	do
S.E. 1,4, N...2, N...6, S...10...					
S...12*.....					
...13, N.E. 25, S.E. 36..	35	7	3	Farley or (c) Grandora, Sec. 19,36,7.....	do
	See Note B				
N.W. 7, S.E. 28, S...7.....	37	7	3	(c) Grandora.....	do
	See Note B				
S.E. 3, S...1, E. 31*...	35	8	3	(c) Hawoods, Sec. 21,36,8.	do
	See Note B				
S.W. 36, ...E. 19.....	36	8	3	do	do
	See Note B				
...1, ...23, ...30, ...	37	8	3	do	do
N.W. 18,31, S.W. 6,18, N.E. 16,20,22					
S.E. 18, S.E. 14*, N...12, S...31...	See Note B				
...7, N.E. 1,35.....	36	9	3	(c) Asquith, Sec. 27,36,9..	do
	See Note B				
...21, ...36, N.W. 15.....	37	9	3	do	do
S.W. 28,32, N.E. 34, S.E. 35.....					
N...16, N...35, S...30, ...E. 20}	See Note B				
...1, ...10, ...16, ...21}	38	9	3	do	do
N.W. 14, S.W. 12, N.E. 18, S.E. 2..					
N...2, N...9.....	See Note B				
N.W. 3*, S.E. 19*, ...W. 32.....	34	10	3	(c) Kinley, Sec. 6,36,10...	do
	See Note B				
N.W. 34.....	35	10	3	do	do
	See Note B				
S.W. 10.....	36	10	3	(c) Juniata, Sec. 14,36,10.	do
	See Note B				
N.W. 16,20, N.E. 18,34.....	37	10	3	do	do
S.E. 16,20,32.....					
N.W. 36, S.W. 28,30,32.....	38	10	3	do	do
N.E. 10,24,32, S.E. 2,4,32, ...E. 16					
...W. 15.....	See Note B				
N.W. 18, S.W. 21,27,32.....	33	11	3	(c) Leney, Sec. 23,35,11..	do
N.E. 21,24,30, S.E. 20,35, N...17..					
S...30.....	See Note B				
S.W. 14.....	34	11	3	do	do
	See Note B				
N.W. 31, S.E. 2.....	38	11	3	(c) Kinley.....	Battleford, Sask
	See Note B				
S.W. 30,31, N.E. 10,13,24.....	33	12	3	(c) Leney or Mead, Sec. 24,35,13.....	Saskatoon, Sask.
S.E. 10,18,24,34, N...31, S...14..					
...W. 3.....	See Note B				
...5, ...6, ...7, ...9.....	34	12	3	(c) Leney or Mead.....	do
...17, ...18, S...19, N.W. 10..					
S.W. 12, S.W. 21*, N.E. 4, S.E. 25..	See Note B				
S.E. 20*, S.E. 25*,30*, N.E. 19....					

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
N.W. 27, 32, 34, S.W. 31, N.E. 5..... N.E. 20*, N...31.....	35 See Notes A	12 Notes A	3 & B	(c) Leney or Mead	Saskatoon, Sask.
.....5,6,7,915,17,18,1921,30,31,32	36 See Notes A	12 Notes A	3 & B	do	do
N.W. 14, 28, N.W. 34*, S.W. 4..... S.W. 24*, N.E. 4, 10, 16, 20, 22..... N.E. 12*, S.E. 27, S...20, S...2213* and 33*.....					
.....4,5,6,716,17,18,2021,30,31,32 S.W. 33, 34, S.W. 10*, 15*, 22*..... N.E. 27, S.E. 14, 23, N...15*..... N...33, N...34, S.E. 3*, ...W.28W. 3*.....	37 See Notes A	12 Notes A	3 & B	(c) Leney or Mead	do
.....3,4,5,67,10,19,31 N.W. 2, 16, 18, 22, 30, S.W. 9, 15, 20, 27 N.E. 9, 20, 21, S.E. 16, N...15..... N...27, S...18, S...21, S...22..... ...E. 17, ...E. 28, ...W. 14..... ...W. 32, ...W. 34.....	38 See Notes A	12 Notes A	3 & B	(c) Leney or Mead	do
.....7,18,20,2527,30,31,3236, N.W. 19, 34, S.W. 19-21-33 N.E. 26, 35, S.E. 6, 17, 22, 28, N...6 N...21, N...23, N...28, N...33..	33 See Notes A	13 Notes A	3 & B	Mead or Neola, Sec. 29, 35, 13	do
.....1,2,3,45,6,7,912,13,14,1516,17,18,1920,21,22,2330,31,32, N.W. 24 S.W. 36, N.E. 10, N.E. 25*..... S...10, S...24, ...W. 33.....	34 See Notes A	13 Notes A	3 & B	do	do
.....4,5,6,79,15,16,1718,19,20,2122,23,25,2730,31,32,3334,35,36, N.W. 3, 13, S.W. 10, S...28, ...W. 14	35 See Note B	13 Note B	3	do	do
Entire township except Sec. 8, 11. N.W. 26, 29, S...26.....	36 See Notes A	13 Notes A	3 & B	do	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
1, 2, 3, 4 5, 6, 9, 10 12, 13, 14, 15 16, 17, 20, 21 22, 23, 24, 25 27, 30, 32, 35 36, N.W. 18, N.E. 26, S. 18. S. 28.	37	13	3	Mead or Neola.....	Saskatoon, Sask.
2, 4, 6, 10 12, 14, 16, 18 20, 22, 24, 28 30, 32, 34, 36 N.E. 26.	38	13	3	do	Battleford, Sask.
2, 4, 10, 12 14, 16, 18, 20 22, 24, 28, 30 32, 34, N.W. 6, S. 6. N.E. 26.	39	13	3	(c) Biggar, Sec. 31, 35, 14...	do.
2, 7, 13, 16 17, 18, 19, 20 21, 22, 23, 24 25, 27, 28, 30 31, 32, 33, 34 35, 36, E. 3, N. 6. S.W. 6, W. 9, W. 15, S. 12. S.E. 14, N.E. 26.	33	14	3	Neola or (c) Biggar.....	Saskatoon, Sask.
Entire except Sec. 8, 11, 29, N.W. 26 S. 26.	34	14	3	do	do
1, 2, 3, 4 5, 6, 7, 9 10, 12, 13, 14 15, 16, 17, 18 19, 20, 22, 23 24, 25, 27, 33 35, N. 21, S.E. 21, N.W. 28, 34, S. 30, S.E. 32, 26, 34* N. 36, N. 34.	35	14	3	do	do
1, 2, 3, 9 12, 13, 16, 21 24, 25, 27, 28 30, 31, 32, 33 34, 35, 36 N.W. 5, 22, N.E. 6*, 26, S.W. 7 W. 10, S. 15, E. 17 N. 20, S.E. 20, E. 23.	36	14	3	do	do
2, 12, 36, E. 4 N.W. 6, 16, 34, S. 6, S. 10 S. 14, W. 24, N.E. 14, 26, 28. S.E. 24, S.W. 32, S. 34.	37	14	3	do	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
.12, .24, .28, .30 .32, N.E. 2, S. 2, W. 6. N. 14, N.E. 6, 22, 26, S. 22. S.E. 14, N.W. 18, 36, W. 20. W. 34.	38	14 See Note B	3	(c) Biggar.....	Battleford, Sask.
.30, .32, .36 N.W. 4, 14, 16, 28, S. 16. S.E. 4, 6, 14, 22, 24, 28, W. 18. N.E. 20, 26, N. 22, N. 24. N. 34, S.W. 34.	39	14 See Note B	3	do	do
Entire except Sec. 8, 11, 29, N.W. 26 S. 26.	33	15 See Notes A & B	3	do	Saskatoon, Sask.
.1, .2, .3, .5 .10, .12, .13, .14 .21, .23, .24, .25 .27, .28, .30, .31 .32, .33, .34, .35 .36, E. 4, N.W. 4, 17, 20. S.E. 6, 7, 22, E. 15, N.E. 26. S.W. 15, 20, N. 16, N. 22.	34	15 See Notes A & B	3	do	do
.1, .2, .3, .4 .5, .6, .10, .12 .13, .15, .16, .19 .20, .21, .23, .24 S. 7, S. 9, N.E. 9, 28, N. 14. S.W. 14, 35, W. 17, N. 18. S.E. 18, 22, 34, W. 22, S. 28. S.W. 31, E. 33.	35	15 See Note B	3	do	do
.9, .19, .21 .25, .27, .33, .34 .35, N.W. 4, 13, 16, 20, S. 4. S.W. 6, 15, 36, S. 16, N.E. 12, 17, 23 S.E. 17, 32, 36, W. 18, N. 28. N. 36.	36	15 See Notes A & B	3	do	do
.1, .2, .3, .4 .9, .12, .17, .18 .19, .22, .23, .25 .27, .30, .31, .32 .33, .35, .36, E. 5 N. 7, W. 10, S.E. 10, 13, 21. N. 14, N. 15, S.W. 15, 16, 34. N. 20, N. 21, N.E. 26, N. 28. S. 28, N. 34.	37	15 See Notes A & B	3	(c) Biggar or Oban, Sec. 30, 36, 15.....	do
Entire except Sec. 8, 11, S. 26. N.W. 26, 36, S.E. 20, W. 28.	38	15 See Notes A & B	3	Oban.....	Battleford, Sask.

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
.....4,6,24, S...2.. S.W. 10, 22, 34, N...16..... N.E. 14, 22, 26, 34, S...36..... S.E. 16, 32, N.W. 22, 30.....	39	15 See Note	3 B	(e) Palo, Sec. 12, 37, 17....	Battleford, Sask.
.....2,36, N...4, S.E. 4... N.W. 6, S...7, S...9,S...10 N.E. 14, 26, S.E. 24.....	40	15 See Notes	3 A & B	(e) Palo.....	do
Entire except Sec. 8, 11, 29, N...1.. ...W. 2, S.E. 2, 3, 24, N...3, S.W. 3 ...E. 12, ...E. 13, S...26; N.W. 26	34	16 See Notes	3 A & B	Oban.....	Saskatoon, Sask.
.....1,2,3,45,9,10,1314,15,23,2427,33,35, N.W. 6. S...6, S...7, S...12..... N.E. 12, 21, 22, 28, 31, S.E. 20..... S...21, N...34.....	35	16 See Notes	3 A & B	do	do
.....3,13,25..... N.W. 1, 16, S...4, S...5, N...9.. N.E. 7, 18, S.W. 9, N...14, N...15, N...17 S.W. 14, 17, 24, N...19*..... N...20*, 21*, 22*, 23*, N...24..... N.E. 26, ...W. 32, 27*, 28*, 30*, 31* S...34, S.E. 32.....	36	16 See Notes	3 A & B	Oban or (e) Palo.....	do
.....19,23,27,3335, ...E. 2, N...6..... N.E. 18, 20, 26, 34, 36, S...18..... N.W. 24, S...24, S.E. 30, ...W. 20. S...34, S...36.....	37	16 See Note	3 B	do	do
.....10,12,13,1415,16,18,2021,22,24,2832,34,36, ...E. 2 ...E. 4, ...E. 6, S.W. 6, N.E. 26, ...30*	38	16 See Note	3 B	(e) Palo.....	Battleford, Sask.
.....4,6,20,32 ...36, N.E. 2, S.E. 18, S. 28.. N...30, S.W. 30, N...34.....	39	16 See Note	3 B	(e) Palo.....	do
.....2,4,6,1016,18,22, S.W. 12 W. 14*, N...20, S.E. 20, W. 23*...	40	16 See Note	3 B	do	do
Entire except Sec. 8, 11, S...4.... S...26, N.W. 26.....	34	17 See Notes	3 A & B	Oban.....	Saskatoon, Sask.

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
1, 4, 5, 6 7, 15, 20, 28 S. 2, N. 2, S. 10, W. 14 S. 16, N.W. 16, 17, 21, 30, S.E. 17 N.E. 17, E. 19, S. 21 W. 23, S.E. 23, S. 30, W. 33	35	17	3	Oban or (c) Landis, Sec. 24, 37, 18	Saskatoon, Sask.
N.W. 12, S.W. 18, 30, N. 20 S.E. 20, N.E. 22, 24*, 28*, 32*, 36* S. 34*, N. 30	36	17	3	(c) Landis	do
W. 4, 6, S.W. 24	37	17	3	(c) Palo or (c) Landis	do
N.W. 12, S.E. 14, S.W. 16 E. 24, 36*	38	17	3	(c) Palo or (c) Landis	Battleford, Sask.
N.F. 1, S.E. 1, N.W. 2, 10 14, 15, 16, 17 19, 20, 21, 22 25, 27, 28, 30 33, 34, N.W. 12, N.W. 13 S. 13, N. 18, S.E. 18, N. 24 N.E. 26, 35*, 36*, S.W. 24, 23	39	17	3	(c) Coblenz, Sec. 12, 38, 19	do
1, 3, 4, 9 10, 12, 13, 14 15, 16, 17, N. 18 19, 20, 21, 22 22, 23, 24, 27 28, 30, 32, 33 34, 2*, E. 6, W. 35 S.E. 18	40	17	3	(c) Colbens	do
19, 20, 24, 25 30, 31, 36, N.E. 26 N.W. 32, S.W. 32	34	18	3	(c) Landis	Saskatoon, Sask.
N.E. 1, S. 1, S. 2, N.W. 2* W. 4, 6, 9, S. 10 S. 13, N.W. 14, 30, N. 19 N. 20, S.E. 20, 32, 21*, N. 25 E. 27, S. 30, N. 32* N.W. 33*, S. 33*, S. 34*, S. 35	35	18	3	do	do
N.E. 6, 26, 30, N.W. 12, S.W. 14 36	36	18	3	do	do
2, 16, E. 10 N.W. 34	37	18	3	do	do
E. 31*, W. 32*, S.W. 21	38	18	3	do	Battleford, Sask.
E. 14, E. 16, S.W. 18*, S.E. 24 22, 24, N.E. 32	39	18	3	(c) Coblenz or (c) Reford, Sec. 5, 39, 19	do
2, 7, 15, N. 9 S.W. 12, 13, 30, N. 13, N. 14 16, N.E. 20, 27, 24, 25 28, 36, S. 27, S.E. 34 16, 17, 23	40	18	3	(c) Reford	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
N.E. 2,4,12, S. 2, W. 10, S. 12 } N.W. 14, S.W. 34, ... W. 36. }	41	18	3	(c) Reford	Battleford, Sask.
	See Note	B			
N.E. 2,4,3,134, N. 12. } S.E. 6,18,24*,27, S.W. 12. }	42	18	3	do	do
W. 24, N. 25, S.W. 25, N. 27, N. 32 }		See Note	A		
.... 1, N. 2, S.E. 2,16,24, N. 3 } N.E. 4,5, S.W. 5, ... 10. 12. }	35	19	3	(c) Landis.....	Saskatoon, Sask.
.... 22, N.W. 14,20, S. 14. }	See Note	B			
N. 16,19*, S. 20*,25*, N. 24. }					
.... 28, 33, 34, 35 }					
.... E. 32, W. 36. }					
S.W. 2,12,16, S. 4, N.E. 4, N. 16 } N.W. 28. }	36	19	3	(c) Landis or (c) Coblenz.	do
	See Note	B			
N.E. 22*, S.W. 24, S. 28*.....	39	19	3	(c) Reford	Battleford, Sask.
	See Note	B			
S.E. 18, N.W. 20, S.W. 24,28*..... } N. 28*...34, N.E. 34..... }	41	19	3	do
	See Note	B			
S.E. 6*,10, N.W. 18, ... W. 20. }	36	20	3	(c) Coblenz or (c) Scott.	Saskatoon, Sask.
N.E. 32, S. 32, ... W. 28. }	See Note	B		Sec. 21,39,20.....	
W. 4*, S.W. 16,20,30, E. 30*..... }	37	20	3	(c) Coblenz or (c) Scott....	do
W. 32*..... }	See Note	B			
S.E. 6,20*, W. 28*.....	38	20	3	(c) Reford or (c) Scott....	Battleford, Sask.
	See Note	B			
N.E. 17*, S.W. 20.....	40	20	3	(c) Scott.....	do
N.W. 4, S.E. 6.....	36	21	3	(c) Scott.....	Saskatoon, Sask.
	See Note	B			
N.W. 32.....	37	21	3	do	do
	See Note	B			
S.W. 18,34.....	38	21	3	do	Battleford, Sask.
	See Note	B			
S.E. 20, N.E. 26.....	39	21	3	(c) Scott or Tako, Sec	do
	See Note	B		31,39,21.....	do
S.W. 16.....	40	21	3	Tako.....	do
N.W. 4,10,13, S. 4, N. 16.....5 }					
.... 6, 7, 9, 15 }					
.... 18, S.W. 12,14,23, 21. }					
S.E. 16,22, N. 22, N. 23. }	37	22	3	(c) Scott.....	Saskatoon, Sask.
17*,19*,20*,30*,31*, 27..... }	See Notes	A & B			
.... E. 28, 32, 33, 34 }					
.... 35					
.... 2, 3, 4, 5 }					
.... 6, 7, 9, 10 }					
.... 13, N. 14, S.E. 14, N. 25 }	38	22	3	(c) Scott or Tako.....	Battleford, Sask.
.... 15, 16, 17, 18 }	See Notes	A & B			
.... 19, 20, 21, 22 }					
.... 23, S.W. 25,36, N.E. 26,35*.... }					
S.W. 1,30,36, N.W. 2, S. 2*, N. 3* }					
N. 4*, N. 5*, W. 6*, 7, ... W. 19 }	39	22	3	Tako or (c) Unity, Sec.	
N.E. 23, W. 35*..... }	See Note	B		18,40,22.....	

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
S.W. 2*, 3*, N. 30*.....	40	22	3	Tako or (c) Unity, Sec. 18, 40, 22	Battleford, Sask.
N.E. 2.....	41	22	3	(c) Unity.....	do
N.W. 18*... W. 30.....	43	22	3	(c) Vera, Sec. 24, 41, 24.....	do
... E. 6, ... W. 16, S.E. 32.....	44	22	3	do	do
N.E. 10, 12, S... 12.....	45	22	3	do	do
..... 1, N.W. 3, 18*, 28, N.E. 4-5, 26	37	23	3	(c) Scott.....	Saskatoon, Sask.
... W. 9, N... 15, 12, 13					
..... 14, N... 16, S.W. 16, S.E. 18					
S... 28, 17*, 20*, 30, 21, 22					
... 23, 24, 25, 27					
... 31, 32, 33, 34	See Notes A & B		3	(c) Scott or (c) Unity.....	Battleford, Sask.
... 35, S... 36.....					
..... 1, 2, 3, 4	38	23	3	(c) Scott or (c) Unity.....	Battleford, Sask.
..... 5, 6, 7, 9					
..... 10, 12, S... 13, 14					
..... 15, 16, 17, 18					
S... 19, 20, 21, 22					
... 23, 24, 25, 30	See Notes A & B		3	(c) Unity.....	do
N.E. 26, N... 31, N.W. 32, 13, 19, ..					
27*, 28*, S. 33*, 36*, S... 34, S... 35	39	23	3	(c) Unity.....	do
..... 1, 2, 12, 13					
..... 15, N... 17, 3*, 14*, 23*.....					
S.E. 4, 7, 10, S... 5, S.W. 6, 10*.....					
N... 18, S... 19*, S... 21.....					
N.E. 22, 28*, S... 22, N.W. 24*.....	40	23	3	(c) Unity.....	do
S. 24*, ... E. 25, S.E. 34.....					
... E. 1, N.E. 14, 22, 25.....					
N.W. 18, 27, 33*, S.E. 22, 30, 23.....					
N... 23, S... 27, N... 28, S.W. 28					
N... 30, 31 32*, S... 33	See Notes A & B		3	(c) Unity or (c) Vera....	do
32*, S... 33, W. 4*-5*, ... E. 6*.....					
... W. 6, 7, 18.....	41	23	3	(c) Unity or (c) Vera....	do
N.W. 19, 20, S... 19.....	See Note B		3	(c) Vera.....	do
N.E. 2, 20, 26, 34, N... 6, S.W. 6.....					
..... 18, S.E. 16, ... W. 20.....	42	23	3	(c) Vera.....	do
N.W. 24, 28, 30, S... 28, S.W. 30.....	See Note B		3	(c) Vera.....	do
... E. 32.....					
N.W. 4, 6, N.E. 12, S... 12	43	23	3	(c) Vera.....	do
..... 16, 18, 20, 22					
N... 24, S.E. 24, 28, 30					
... 32.....					
N... 4, S.W. 4, 6, 16					
..... 18, 20, N... 22, S.E. 22	44	23	3	(c) Winter, Sec. 15, 42, 25...	do
N.E. 26, ... W. 28, ... 30, 32					
... 34, N.W. 36, 38.....					

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
S...2, N.W. 2, 12, S...4, N.E. 4...6,10, S.W. 14,1618,20,22, N...2428, S.E. 30*,32,3436	43	23	3	(c) Winter.....	Battleford, Sask.
.....1,2,3,45, S.E. 9,W. 10.....12,13,14,1516,E. 17,20,2122, N...23, S.E. 23,2425,27,28,3031,32, N.W. 33, S...33 N.E. 34, S...34,35,W. 36	38 See Notes A & B	24	3	(c) Unity.....	do
.....1, S.W. 2, 4, 15, 25, 35,3 N...4,5,6,7 S...9, N.E. 10, N.W. 24*, 34*E. 33.....	39 See Notes A & B	24	3	(c) Unity.....	do
N...6, S.W. 6, 9, 32, N...9.....7,15,18..... 16*, 17*, 20*, 21*, 27*,19...22,28, N...23, S.E. 23... N...32,30,31,3334,35,36.....	40 See Notes A & B	24	3	do	do
Entire except 8, 11, 29,S.E. 2...	41 See Notes A & B	24	3	(c) Vera.....	do
Entire except Sec. 8, 11, 29, N.W. 26 S...*26.....	42 See Notes A & B	24	3	(e) Vera or (c) Winter.....	do
.....2,3,4,56,7,9,1012,14,15,1617,18,19,2021,22,23,2427,28,30,3234,36, N.E. 26,E. 31	43 See Notes A & B	24	3	(c) Winter.....	do
Entire except, Sec. 8, 11, 29, N.W. 26 S. 26.....	44 See Note B	24	3	do	do
.....2,4,S...6,1012,14,16..... N.E. 18, 32, S...18,E 20...22,28, S.W. 24, S...32... N.E. 6.....	45 See Note A	24	3	do	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
N.E. 2, 26, S.W. 2, N... 4, ... E. 6... N... 10, N... 14, N... 24, ... 16 S... 18, ... 20, ... 25, ... 27 ... 28, ... 30, ... 32, ... 33 ... 34, ... 35, S... 36, N.W. 36 ... 22, ... 23, S.E. 24, N.W. 18	46	24 See Note A	3	(c) Winter.....	Battleford, Sask.
... 1, ... 2, ... 3, ... 4 N.E. 5, 26, S.W. 5, S... 6, ... W. 7... ... 9, ... 10, S... 12, ... 15 ... 16, 14*, ... 17, ... 18... ... 19, ... 20, ... 21, ... 22 ... 23, ... 27, ... 28, ... 30 ... 31, ... W. 32, ... 33 ... 34, ... 35, N... 36, N.W. 6	39	25 See Notes A & B	3	(c) Vera.....	do
Entire except Sec. 8, 11, 26, 29.....	40	25 See Notes A & B	3	do	do
... 1, ... 2, ... 3, ... 4 ... 5, S.W. 7, N... 10, S... 12... N.E. 12, 26, ... 13, ... 14... ... 15, ... 16, N... 17, S.E. 17... ... 19, N... 20, ... 21, S... 22 N.W. 22, ... 23, ... 24, ... 25 ... 27, ... 28, ... 30, ... 31 ... 32, ... 33, ... 34, ... 35 ... 36.....	41	25 See Notes A & B	3	(c) Winter.....	do
Entire except Sec. 8, 11, 29, S. 26... N.W. 26, W. 14, E. 15, S.E. 25.....	42	25 See Notes A & B	3	do	do
... 1, ... 2, ... 3, ... 4 ... 5, ... 6, S... 10, N.E. 10... ... E. 12, ... E. 13, N.W. 16, S... 16 N... 20, S.E. 24, ... 25, S... 30... S.W. 36.....	43	25 See Note B	3	do	do
... E. 2, ... E. 10, ... 12, ... 14 N... 16, S.W. 18, S.E. 20, ... 22... ... 24, N.E. 26, N.W. 28, 32... S... 28, ... 34, ... 36.....	44	25 See Note B	3	do	do
... 2, ... 4, ... 10... ... W. 12, S.W. 14, N.W. 14... ... 16, ... 18, ... 20... ... W. 22, ... W. 24, ... 28... ... 30, ... 32.....	45	25	3	do	do
Entire except Sec. 8, 11, 26, 29..... S.W. 6, N.E. 30, S.W. 31, S. 32... N.W. 32, N.E. 32*, E. 34, N.W. 35... N.W. 1a, N.W. 4.....	40	26 See Notes A & B	3	(c) Winter.....	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
N.W. 1, 6, 22, ... W. 3, N. ... 4 S.E. 4, 5*, 10*, 18, 19, N.E. 5, ... S. 6 ... 7, N. ... 9, ... W. 10 ... E. 12, S.W. 14, 24*, 36*, S. ... 15 N.E. 15, 21, 26, 31, S. ... 16, N. ... 18 N. ... 19, W. ... 23, ... E. 24 ... 25, ... 27 N. ... 28, ... W. 30, S. ... 31, ... 32 ... 33, ... 34, ... 35	41	26	3	(c) Yonker, Sec. 22, 42, 26...	Battleford, Sask.
Entire except Sec. 8, 11, 29 N.W. 22, 26, S.E. 25, S. 26	42	26	3	do	do
1*, 3*, 10*, 12*, 13*, 17*, 4, ... 5, ... 6, ... 8 ... 9, S. ... 7, N.E. 7, S.E. 18	43	26	3	(c) Zumbro, Sec. 7, 43, 26...	do
N.W. 15*, S. ... 16, S.E. 22, 17*, S.W. 24 N.E. 26, W. 30	44 45	26 26	3 3	(c) Artland, Sec. 7, 43, 27... do	do do
S. ... 2, N.E. 2, 16, 26, 30 S.W. 6, 32, S. ... 12, N.W. 12 ... 18, ... 28, ... E. 20 N.W. 20, ... W. 22, N. ... 34 S.E. 34, ... 36	46	26	3	(c) Artland.....	do
... 1, N.E. 2, ... 3, ... 4 ... 5, ... 6, ... 7, N. ... 10 S.E. 10, ... E. 12, ... W. 14 ... 9, ... 15, ... 16, ... 17 ... 18, ... 19, ... 20, ... 21 ... 22, ... 23, N. ... 24, ... 25 N.E. 26, ... 27, ... 28, W. ... 30 ... 31, ... 32, ... 33, ... 34 ... 35, ... 36	41	27	3	(c) Zumbro.....	do
Entire except Sec. 8, 11, 29, S. 26 N.W. 26	42	27	3	(c) Zumbro or (c) Artland	do
1, ... 2, ... 3, ... 4 5, ... 6, S. ... 7, N.W. 7, 12 9, 10*, 15*, 36*, S. ... 12 N.E. 13*, S.W. 13*, ... 16 N. ... 17, S.E. 17, N.E. 18, 26 W. 18, ... 19, ... E. 20, ... 21 22, ... 23, ... 24, ... 25 27, ... 28, S. ... 34	43	27	3	do	do
N.W. 2, 18, 30, 32, S.W. 4 N. ... 10, S.W. 10, S. ... 15 12*, 13*, 14*, N.E. 15, S. ... 16 N. ... 22*, E. 28*, S. ... 30, N. 34* S.W. 34*	44	27	3	(c) Artland.....	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
4*, 6, N. 10, S.W. 10 15, S. 16, N.E. 16, 30. 17, 18, 20, 21 22, 28, S. 30, 32 34, N.W. 36.	45	27	3	(c) Artland.....	Battleford, Sask.
..... 2, 4, 10, S. 6 12, 14, N. 16. S.E. 16, 34, N.E. 18, 26, 20. 22, 24, S. 28, N. 36. S.W. 36.	46 See Note B	27	3	do	do
..... E. 1, N. 2, S.E. 2, N. 3. N.E. 4, 15, E. 5, W. 5*, 9. 10, 12, 13, 14 16, S. 15, E. 17, W. 17*.. E. 20, W. 20*, 21, 22 23, 24, 25, N.E. 26 27, 28, E. 32, W. 32*.. 33, 34, 35, 36	41 See Notes A & B	28	3	do	do
Entire except Sec. 8, 11, 29, N.W. 26 S. 26.	42 See Notes A & B	28	3	do	do
1*, 3*, 4*, 10, 13, 14 E. 16*, 22, N.E. 24, 26. S. 24, E. 28, 34.	43 See Notes A & B	28	3	do	do
..... 2, 10, 12 N.E. 4, 26, E. 16*, 14, N. 22 S.W. 22, 24, E. 28. 34, 36.	44 See Note B	28	3	do	do
..... 2, 10, 12, E. 4 E. 16, N. 22, S.E. 22, 14 24, 36, N.E. 28, N.W. 34 S. 34.	45	28	3	do	do
..... E. 4, E. 16, E. 28.	46	28	3	do	do
..... 2, 4, 10, E. 6*.. 12, 14, 16, 20 22, 24, 25, 28 N.E. 26, 27, 30, S. 30, E. 35.. 31*, 32*, 33*, 34*, W. 35*, 36..	41 See Note B	1	4	(c) Artland or Butze, Sec. 10, 43, 1.	Red Deer, Alta.
..... 1, 2, 3, 4 6, 7, 5*, 9*, S. 10.. 12, 13, 14, 16 S.W. 18, W. 20, 22, 23 24, N.E. 26, N. 28, 30. 32, 34, 35, S.W. 36* N.E. 10.	42 See Notes A & B	1	4	Butze.....	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
1*,2,4,5.....6,10, S.E. 7, 17, N. 17 N. 18, S. 22,12,14..16,19,20,24 N.E. 26,S.W. 32,S.W. 3628,30,34, N. 35 S.E. 35, N.E. 34,N.}	43	1	4	Butze or Chauvin, Sec. 7, 43, 1.....	Edmonton, Alta.
N.W. 2, 10, 16, S.E. 6, 18, 22,1219,20, N. 18, N. 2221, S.W. 24,W. 34, S.E. 3628,30,32,}	44	1	4	Butze or (c) Chauvin.....	do
.....E. 2, N. 4, S.E. 6, S. 14.....10,12, N.E. 14, 24, 28.. S.W. 16, N. 18, S.E. 18 S. 24, N. 36.....}	45	1	4	do	do
N.E. 2, 24, 36, N.W. 14..... S.E. 10, 18,12, S. 14.....E. 22,W. 28.....}	46	1	4	do	do
S.E. 4, 20,6, N.W. 10..... S.W. 12,W. 16,E. 18.....}	47	1	4	do 1	do
N.W. 2, N. 4, S.E. 4,3.....6,7,9,1016,18,20,22 S. 14, N. 24, N.E. 26, N. 30..27,28,S. 32,3436,N.W. 32.....}	41	2	4	(c) Chauvin.....	Red Deer, Alta.
.....1,2, S. 4, N.E. 4..... S. 10, S. 12, N.E. 12, N. 14.. S.E. 14, S.W. 18,E. 20,W. 2416,S. 22,25,2830,31,E. 32.....34,E. 36, N.E. 22.....}	42	2	4	do	do
.....1,2,4,6.....7, S.E. 10, 28, N.W. 10, 22, 36..W. 16, S.E. 18,17, S.W. 20 S. 22,E. 24,34.....}	43	2	4	do	Edmonton, Alta.
S.E. 2, 34, 36, N.E. 6, N. 10..... N.W. 18, 28, 34, N.E. 24..... N.E. 30, 32, S. 32.....}	44	2	4	Chauvin or (c) Dunn, Sec. 21, 43, 3	do
S.E. 18, 20, N. 20,24.....28,30, S. 34, N.W. 34.....}	45	2	4	(c) Dunn.....	do
S.W. 4, N. 6, S.E. 6, 14, 22.....N.E. 18, 26, N.W. 28.....24.....}	46	2	4	(c) Edgerton, Sec. 36, 43, 4.	do
N. 2, N.W. 6, S. 10, N.E. 10.....14,18,22,W. 24, S.W. 28, S. 30.....34, N. 36, S.E. 36.....}	47	2	4	do	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
2, 4, 6 } S.W. 6, 34, N.W. 10, 14, S. 16 } N.E. 16, 18, 22 } W. 20, 28, 30, S. 32 }	41	3	4	(c) Dunn.....	Red Deer, Alta.
	See Note B				
N.W. 2, S.W. 6, S. 4, N.W. 4, 24 } N. 6, 7, 9, 10 } 12, 14, 17, 18 } 19, S. 20, S. 24, 23 } 25, 27, 28, N.E. 26 } 31, 33, 34, 35 } 36 }	42	3	4	do	do
	See Notes A & B				
1, 2, 4, 5 } 6, 7, 9, N. 10 } S.E. 10, 27, N.W. 12, 13 } 14, 15, 16, 17 } 18, 19, 20, S. 21 } N.W. 21, S.W. 22, 23, N. 27 } S.E. 27, N. 28, S.W. 23, 30 } 31, 32, 33, 34 }	43	3	4	(c) Dunn or (c) Edgerton...	Edmonton, Alta.
	See Notes A & B				
S.W. 2, 30, 36, 4, 5 } S.E. 6, N.W. 6, 12, 30, S.E. 14 } N. 22, S.E. 26, S.W. 32 }	44	3	4	do	do
	See Note B				
S.W. 2, 10, 22, 32, 6, N.E. 18 } N.W. 20, 36, S.E. 20, N. 24 } 28, S. 30, N.E. 32, 34 } S. 36, S. 18 }	45	3	4	(c) Edgerton.....	do
S.E. 2, 4, 6 } 10, 16, S.W. 14, 20 } 22, N.E. 26, 28, 30 } 32, 34, 18 }	46	3	4	do	do
N. 2, S.W. 2, 4, 6 } 10, 16, N.W. 12 } S.E. 14, 18, 20, 22, N.W. 18 } S.E. 24, 30 }	47	3	4	do	do
2, 4, 5, 6 } 7, 10, 12, 13 } 14, 16, 17, 22 } S.E. 18, N.E. 26, 24, S. 31 } N.W. 32, 36, N.W. 18 }	41	4	4	(c) Edgerton.....	Red Deer, Alta.
	See Note B				
N. 6, S.E. 12, 20, 13 } N.W. 14, 28, 18, N. 20 } S.W. 20, 22, 36, N.E. 26, 23 } 24, 25, 27 } 30, 31, 32, E. 36 } S.W. 6 }	42	4	4	do	do
	See Note B				
N. 4, S.W. 4-24, 6, 7 } N.W. 10, 12, 13, 16 } 17, 18, 19, 20 } 25, N.E. 24, 30, 31 } 32, S.E. 36 }	43	4	4	(c) Edgerton.....	Edmonton, Alta.
	See Note B				

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
N.E. 1, 16, 20, 21, 30 ... N.W. 32 } N. ... 4, S.W. 4, ... 5, N. ... 6 } S.E. 6, 14, 18, ... 7, ... 10 } ... W. 14, S. ... 16, ... 17, N. 18 } ... 22, S. ... 28, S. ... 32, ... 34 } N. 36, S.E. 36 }	44	4	4	(c) Edgerton or Heath....	Edmonton, Alta.
	See Note B				
S.W. 2, 16, 20, 24, 32, S. ... 6 } N.E. 6, 10, N. ... 12, S.E. 12, 14, 24 } N. ... 14, ... 18, N. ... 32, ... 36 }	45	4	4	Heath, Sec. 13, 44, 5, ...	do
... E. 2, ... E. 4, S.E. 6, N. ... 6 } N. ... 12, ... 10, ... 14 } S.W. 12, ... 16, ... 18 } ... 20, ... 22, ... 24, ... 28 } N.E. 26, N.E. 32, ... 30, ... 34 } ... 36 }	46	4	4	do	do
... 2, ... 4, N. ... 6, S.W. 6 } ... 10, ... 12, ... W. 14 } ... 16, ... 18, ... 20, ... 22 } ... N.E. 26, ... 30, ... N. 32 } ... 33, ... 34, ... 36, S.W. 32 }	47	4	4	do	do
... 1, ... 2, ... 3, ... 4 } ... 5, ... 6, ... 7, ... 9 } ... 10, ... 12, ... 13, ... 14 } ... 15, ... 16, ... 17, ... 18 } ... 19, ... 20, ... 22, ... 23 } ... 24, ... 25, ... 27, N. 28 } N.E. 26, ... 30, ... 32, ... 33 } ... 34, ... 35, ... 36, S.W. 28 }	42	5	4	do	Red Deer, Alta.
	See Notes A & B				
... 2, ... 3, ... 4, ... 5 } ... 6, ... 7, ... 9, ... 10 } ... 12, ... 13, ... 14, ... 15 } ... 16, ... 17, ... 18, ... 19 } ... 20, ... 22, ... 23, ... 24 } ... 25, N.E. 26, 27, S. ... 27 } S. ... 28, S. ... 30, N.W. 30, ... E. 34 } ... 35, ... 36 }	43	5	4	do	Edmonton, Alta.
	See Notes A & B				
1*, 2*, N.E. 4*, ... 12, 10* } ... 13, ... 15, N.E. 14, 20 } N.W. 16, 28, 36, S.E. 16, 22, ... 23 } ... W. 24 }	44	5	4	Heath, or (c) Wainwright, Sec 31, 44, 6	do
	See Note B				
N.W. 2, ... E. 4, ... 10, N. ... 16 } N.E. 12, 18, 22, 32, S.E. 16, 28 } ... 20, S. ... 22, S.W. 24, 30* } N. ... 28, N. ... 34 }	45	5	4	do	do
S. ... 2 ... S.E. 32, ... S.W. 6 } N.E. 10, 12, 22, 24, N. ... 34 } ... 36 }	46	5	4	(c) Wainwright	do

Canadian Government Lands—Continued

SECTION	T'slip.	R'go	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
.....2, N....4, N.W. 6, S....10...12,14, N....16, S.W. 16. S....18, N.E. 18, 26, N....20, S....20 N....24, S.E. 24,22,2830,32,34,36	47	5	4	(c) Wainwright	Edmonton, Alta.
.....12,14,N. 16...20,22,23,2425, N.W. 18, S.E. 18, S.W. 18.. S.E. 32,28, N.E. 26, 32..... S. 34, S. 36, N.E. 36, 34.....	43	6 See Note B	4	(c) Greenshields, Sec. 14, 44, 6	do
S.W. 6.....	44	6 See Note B	4	(c) Greenshields or (c) Wainwright	do.
S. 36*.....	45	6	4	(c) Wainwright	do
S....2, S.E. 4, 10, 14,W. 6..... N.W. 18, S....20, N.E. 20,2228,30,32,34 N....36	46	6	4	(c) Wainwright	do
.....2, S.E. 4, N.W. 6, N.E. 34.12, S....36	47	6	4	do	do
N.W. 12.....	44	7 See Notes A & B	4	do	do
S.W. 6, 20, N.E. 18, 20, 28, N.W. 18 S....28, S.E. 30, 36, N....34, N....36	45	7	4	Fabyan, Sec. 17, 45, 7	
S.E. 2, 24, S.W. 2, S....12, N.W. 12, 30, N.E. 24, 26,34..36	46	7	4	Fabyan or Hawkins, Sec. 16, 45, 8	do
.....W. 10, S.W. 12, 16, S.E. 14.....E. 18, N.E. 26, 36	47	7	4	Hawkins	do
.....3, N.W. 4.....	43	8 See Notes A & B	4	(c) Irma Sec. 27, 45, 9	do
S....18, N.E. 18, 33, N....20, S.W. 2021,W. 22, N.W. 28, 34,S.W. 33,32	44	8 See Notes A & B	4	Hawkins	do
S.E. 4, 10, N....10,13, N.E. 18 N....24, S.W. 24, S....32, N.W. 28*	45	8	4	Fabyan or Hawkins.	do
S.E. 28, S.W. 36.....	46	8	4	Hawkins or (c) Irma, Sec. 27, 45, 9	do
.....2,3,4,N. 5 S.W. 6, 7, 22, 32, N.E. 6, 20..... N.W. 6, 7, 12, 28,9,1016,18,19W. 20, N....22, N....24, S.E. 2430, S.E. 5, S. 17, N.W. 17	44	9 See Note B	4	Hawkins or (c) Irma	do
N.E. 13, 15,W. 16,E. 17 S.W. 18, 20, 36*, N....20..... N.W. 30, S....30	45	9	4	(c) Irma	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
N.6, N.W. 10, 16, S.W. 18. N.E. 24, S.E. 28, 30 N.W. 32, S.E. 6	46	9	4	(c) Irma	Edmonton, Alta.
N . . . 4, 6, N.W. 10, . . . 16 . . . 18, 20, . . . 22, S.E. 24 S.W. 24, 28, . . . 30, . . . 32	47	9	4	(c) Irma or (c) Junkins, Sec. 4, 46, 10	do
N.E. 2, 12, . . . S. 13, N. . . 14 S.W. 14, N.W. 16, 31, 36, S.E. 22, . . . S. . . 24, N.E. 24, 25, E. 36*, N.E. 13	44	10	4	(c) Irma	do
N.E. 2, 4, 16, S.W. 2, 6, 10, S. . . 9 . . . N.W. 20, 24	45	10	4	(c) Irma or (c) Junkins	do
S. . . 2, N.E. 6, 10, 30, S.W. 6 S.E. 32, 36, 12, 14 24, N.W. 18, N. . . 36	46	10	4	(c) Junkins	do
. . . W. 4, N.E. 10, 12, 26, S. . . 12 . . . N . . 16, S.E. 16, 32, N.W. 20, 30 . . . S. . . 22, . . . 24, S. . . 30, . . . 36 . . .	47	10	4	(c) Junkins or (c) Kinsella, Sec. 27, 46, 11	do
N . . . 2, S.E. 2, 12, 28, N.E. 4, 26 . . . N . . 6, N. . . 12, . . . 10, . . . 14 16, . . . 18, . . . 22, . . . 24 . . . N . . . 28, . . . 32, . . . 34, . . . 36	48	10	4	(c) Kinsella	do
N.W. 2, S.E. 10, N.E. 30, 34, N. . . 32 S.W. 32, 34	45	11	4	do	do
. . . . 2, 4, 6 N.E. 10*, 26, 32, 34, S. . . 10 N.W. 10, 18*, 20*, S.E. 18, 30, S.E. 34, 36	46	11	4	do	do
N.E. 2, 4, 22, 32, S. . . 4 S.E. 2, 22, 30, 34, 36, W. 10 N.W. 4, 6	47	11	6	(c) Kinsella or (c) Philips, Sec. 12, 47, 12	do
N . . . 2, S.E. 2, 20, 28, 34 N.W. 10, 14*, 28, 34, S.W. 12 16, . . . 22, N.E. 24, 26, 30 36	48	11	4	(c) Philips	do
N.E. 22, 36, S.E. 32, S.W. 36	45	12	4	(c) Kinsella	do
N.W. 2, N.E. 4, 26, 30, N.E. 14 S.E. 12, 16, 24, . . . N.W. 22	46	12	4	(c) Philips	do
N.E. 6, N.W. 34	47	12	4	(c) Philips or (c) Meighen, Sec. 36, 47, 13	do
N.W. 4	48	12	4	do	do
S.W. 36	46	13	6	(c) Meighen	do
S.E. 2	48	13	6	(c) Meighen, or (c) Nestor Sec. 13, 48, 14	do
S.E. 2	47	14	4	(c) Nestor	do
S.E. 22	48	14	4	(c) Nestor or (c) Bruce, Sec. 30, 48, 14	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'go	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
S.E. 32	47	15	4	(c) Bruce.....	Edmonton, Alta.
S.W. 20.....	47	16	4	do.....	do
N.E. 20	50	16	4	(c) Holden, Sec. 14, 49, 16, or (c) Poe, Sec. 30, 49, 16	do
S.E. 34	47	17	4	(c) Poe	do
N.W. 36	48	17	4	(c) Poe or (c) Ryley, Sec. 4, 50, 17	do
N.E. 18*, S.E. 30.....	51	18	4	(c) Tofield, Sec. 1, 51, 19	do
N.W. 36*	52	18	4	do	do
S. 30, N.W. 28, 30.....	51	19	4	(c) Tofield.....	do
S.E. 22, N.W. 24.....	52	19	4	do	do
.... 6, S.E. 20	53	19	4	do	do
S.E. 16*, N. 16, N.E. 20*, 26.....	49	20	4	do	do
N. 28, S.W. 36					
N.E. 4, N.W. 6, 18, ... W. 22, S.E. 24 N. 32.....	50	20	4	do	do
N.E. 2, 19*, N. 4, S.E. 4.....					
.... 5, S.W. 6, 19*, 22.....	51	20	4	(c) Deville, Sec. 32, 51, 20	do
S.W. 4*, S.E. 14, N.W. 32*.....	49	21	4	(c) Tofield.....	do
N.E. 32, 34, N. 36.....					
2*, 27*, N.E.*	50	21	4	(c) Deville..	do
N. 14, S.W. 14, 20*					
N.W. 16, 24, S.E. 16, 31*					
S. 19*, S. 21*, N. 22*.....					
.... 23, S. 24, 25, 27*					
S.E. 31*, 35, 36.....					
N.E. 1, 36*, S.E. 3, N.W. 6, S.W. 25*	51	21	4	do	do
N. 36*	53	21	4	do	do
N.E. 12*, S.E. 12, 16*, N.E. 28.....	50	22	4	do	do
S.W. 3, 18*, N.E. 6, S.E. 9, 16.....	51	22	4	Uncas, Sec. 20, 52, 21, or (c) Ardrossan, Sec. 3, 53, 22	do
S.W. 12, N.W. 14, S.E. 24.....	52	22	4	(c) Ardrossan.....	do
S.W. 7.....	53	23	4	Clover Bar, Sec. 16, 53, 23.....	do
N. 30.....	52	24	4	Edmonton.....	do
E. 1*, N.W. 1, 10, N.W. 3, ... W. 4 S.W. 5, 13, 35, N.E. 5, 26, N. 8*	51	26	4	(c) Spruce Grove, Sec. 3, 53, 27.....	do
.... 9, 14, 15, 16					
.... 17, 18, 19, 20					
.... 21, 22, E. 23, S.E. 27					
N. 28, 30, S. 3.....					
28*	53	26	4	do	do
N.E. 28, S. 28*, N.W. 32, 34.....	50	27	4	do	do
S.W. 2, 33, 3, N.W. 4, 9, 21, 35 N. 5, 6, 10, N. 12 13, N.E. 23, 24, 26, S. 23	51	27	4	do.....	do
S. 24, 25, N. 33.....					
N.E. 27, 36, S.E. 27, 34, S. 35.....					
S.E. 5.....					

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
N.E. 24*, S.E. 20.....	53	27	4	(c) Spruce Grove.....	Edmonton, Alta.
... W. 8, S. ... 18, N.W. 18, ... W. 20 } S. ... 30, N.E. 30.....	54	27	4	do	do
1*.....	51	28	4	do	do
24*, S. 36*.....	54	28	4	do	do
S.E. 20, 28, N.W. 10, 18, 26 } N.E. 20, 24, 30, N. ... 28, ... 36.....	50	1	5	Stoney Plain.....	do
S.W. 8, 18.....	51	1	5	do	do
S.W. 30.....	52	1	5	do	do
N.W. 22, 34, S.E. 22, N.E. 30, S. ... 32 } S. ... 34, N. ... 36, S.W. 36.....	53	1	5	do	do
S.E. 2, 32, S.W. 10, N.E. 14, 32..... } N.W. 28.....	54	1	5	do	do
N.E. 14*, 20, 26, S.E. 15, 33 } N.W. 15, 27, ... 21, ... 23..... } ... W. 24, ... 28, N. ... 30, N. ... 33 } ... 31, ... W. 35.....	55	1	5	do	do
N.W. 12, 30, S.W. 14, 22, 36..... } N.E. 22, 32.....	50	2	5	(c) Carvel, Sec. 34, 52, 2.....	do
S.E. 4, ... 6, S.W. 8, 10.....	51	2	5	do	do
S.W. 16.....	52	2	5	do	do
S.W. 2, 6, 32, S.E. 4, ... E. 6..... } N.W. 24, ... E. 26.....	53	2	5	do	do
... 6, S.E. 16, 32, W. ... 18..... } S. ... 20, N.W. 20, S.W. 22, ... 30 } N.E. 34.....	54	2	5	do	do
N.E. 14, N.W. 24, 26, 34, S.E. 26..... } ... 28, ... 30, ... 31, ... 32 } ... 33, ... 35.....	55	2	5	do	do
S.W. 4, S.E. 22.....	50	3	5	Duffield.....	do
... 1, N.W. 2, 26, S. ... 2, ... 3 } ... 4, S. ... 5 } N.E. 5, 6, 7, 16, 19, 20, 30, S.E. 8..... } ... 9, S. ... 10, ... N.E. 10 } S.W. 30, 32, ... 14, ... 15 } S. ... 16, S. ... 19, ... 22, ... 23 }	51	3	5	do	do
N.E. 12, ... 30.....	52	3	5	do	do
N.E. 22, 34, N. ... 30, S.W. 30, 34, 36.....	53	3	5	do or (c) Wabamun...	do
... E. 2, S.W. 2, S. ... 4, S.E. 10, 24 } ... 12, N.E. 14, 36, N.W. 16, 24.....	54	3	5	do	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
N. 2, S.W. 2, 12, S. 7, N.W. 7 S. 8, 9, N.E. 10, 16, N.W. 12 S.W. 12, 14, 17, 18 19, N. 20, 21, 22 23, 24, 25, 26 27, 28, 30, 31 32, 33, E. 34, 35 36	55	3	5	Duffield.....	Edmonton, Alta.
2, 4, 6 N.E. 10, 27, S. 10, 12, N. 19 24, S. 27, N. 30 S.E. 30, 32, 34, 31, N. 32 33, N. 34, N.W. 36, S. 36 N.W. 4, 6*, 14 N.E. 6, 20, 8 18, 30, S.W. 32, 36	50	4	5	(c) Wabamun, Sec. 11, 53, 4	do
N. 2, S.E. 2, S. 6, N.W. 8 N.E. 10, N. 12, S.W. 12, 20	51	4	5	do	do
N.W. 18, 36, N.E. 30	52	4	5	do	do
N. 2, S.W. 2, 12, 16, N.W. 6, S. 8 10, N. 12, 17, N. 18 N.E. 19, S.E. 20, N. 20, S.E. 22, N.W. 26, N.E. 28, 36	53	4	5	do	do
Entire except, 11, 29, N.W. 1*	54	4	5	do	do
Entire except S.W. 7, S.E. 18, S.W. 18 N.E. 7, 14, N.W. 7, 14, N.W. 18	55	4	5	(c) Fallis.....	do
1, 2, 3, 4 5, 6, 7, 8 9, 10, 11, 12 13, 16, 17, S. 14 N. 15, S.W. 15, 23, 18 19, 20, 21, N. 22 N. 23, N.W. 24, S.E. 24, 25 E. 26, 27, W. 30 31, N. 33, S.W. 33, 35 36	56	4	5	do	do
2*, N.W. 4, S.E. 4, 16, E. 10 N.E. 8, 22, W. 16, 14 N. 20, 24, 26, 28 30, 32, 34, 36 2, 4, 6, 8 10, 16, N. 12 S.W. 12, 26, N.E. 18, W. 24	50	5	5	(c) Wabamun.....	do
	51	5	5	do	do
	52	5	5	(c) Fallis.....	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LAND OFFICE
S.W. 16, N. ... 20, N.E. 30, S.W. 30...	53	5	5	(c) Fallis, Sec. 15, 53, 5.	Edmonton, Alta.
S.E. 10, 14, 20, 26, 16, 18. S. ... 22, N.W. 24, N. ... 26, 28. S.W. 30, 32, 34.....	54	5	5	(c) Fallis.....	do
..... 2, 4, 6, 8 S. ... 10, N.W. 10, S. ... 12, 14.. N. ... 16, 18, N. ... 20, S.W. 20.. 22, 24, 26, E. 28 30, 32, 34, 36	55	5	5	do	do
..... 1, 2, 3, 4 5, 6, 7, 8 9, 10, N.E. 14, 31	56	5	5	do	do
S. ... 14, 15, N.W. 16, 22.....					
S. ... 16, N. ... 23, S.E. 23, E. 24.. 17, 18, 19, 20					
..... 21, S.W. 22, 25, 26					
N. ... 27, 28, 30, N.E. 31 S.E. 31, 32, 33, S. ... 34.. 35, 36, S.W. 27.....					
..... 1, S.W. 2, N.W. 3, S. ... 3..... 4, 5, 6, 7 8, 9, N.W. 10, N.W. 12 S.E. 12, 13, S. ... 14, N.W. 14.. 15, 16, 17, 18 19, N. ... 20, S.W. 20, 21.. S. ... 22, N.E. 22, 23, N. ... 24.. S.E. 24, 25, S.E. 26, 27.. S.E. ... 28, S. ... 30, 31.. S. ... 32, N.E. 34, 32, 35, 36	50	6	5	(c) Gainford, Sec. 22, 53, 6 .	do
N.W. 3, N. ... 4, S.E. 4, 6..... 7, 8, 9..... W. 10, N.W. 16, 17..... 18, S. ... 19, S. ... 20, N.E. 20.. N. ... 24, N. ... 26, N.W. 28, N. ... 30.. 32, N.W. 34, S. ... 34, 36.	51	6	5	(c) Gainford.....	do
..... 2, 4, 6, 8 S. ... 10, N.W. 10, S. ... 12, N.E. 12.. N.W. 12, 14, 16, 18 S. ... 20, S. ... 22, N.W. 22, S.W. 24.. S. ... 26, N.W. 26, E. 28 30 32, S.E. 34.....	52	6	5	do	do
S.W. 4, N. ... 6, S.W. 6, N.E. 8..... S. ... 12, N.E. 12, W. 16, N.E. 24.. N.W. 28, N.W. 34.....	53	6	5	do	do
N. ... 4, S.E. 4, N.W. 8, S.E. 8 10, 12, 14, N. 16.. E. 20, 22, N. ... 24, S.E. 24.. 26, S. ... 28, N.E. 28, W. 30 N. ... 32, S.W. 32, 34, 36.	54	6	5	do	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
N...2, S.E. 2, 4, 5.....6,7,8,9E. 10, S.W. 10,12, 14 S...15, N.E. 15, S...16, N.W. 16..17,18,19, N... 20 S.W. 20,W. 21,W. 22.....23,24,25, N... 26 S.E. 26,27, N...28, S.W. 28.. S...30, N.E. 30,31,3233, N...34, S.E. 34,3536.....	55	6	5	(c) Gainford.....	Edmonton, Alta.
.....1,2,3,45,6,7,89,10,11,1213,14,15,16E. 17, S.W. 17, N.W. 18, S... 18 N.W. 19,E. 20, S.W. 20,2122,23,24,2526,27,28,E.3031,32,33,3435,36.....	56	6	5	do	do
.....1,2,3,45,6,7,89,10,11,1213,14,15,1617,18,19,2021, N...22, S.E. 22,2324,25,26, N... 27 S.E. 27, N...28,29,3031, S.E. 32,33,35 N.E. 34, S...34,36, S.W. 28	52	7	5	(c) Hargwen, Sec. 21, 53, 7..	do
.....1,2, N...3, S.W. 4..5,6,7, N.E. 8.. N.E. 9, S...9, S...12,W. 13.. S...17, N.W. 18, S.W. 19, S.W. 23..W. 24,25.....	53	7	5	do	do
N.E. 10,31,32,N. 3334,35,36, S.E. 33	54	7	5	(c) Hargwen.....	do
Entire except Sec. 11, 29, S.E. 26.... N.W. 30.....	55	7	5	do	do
Entire except Sec. 11-29..... S.W. 12, 24, 26, S.E. 25, 27, N.E. 22, 25 N. 24, 26, S.E. 27,E. 36.....	56	7	5	do	do
Entire except Sec. 11, 29..... N.W. 30.....	52	8	5	(c) Hargwen and (c) Isfield Sec. 22, 53, 8.....	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
1, 2, 3, 4 5, 6, 7, 8 N.W. 8, S.W. 10, 9, 12 13, 15, N. 17, S.W. 17 N.E. 18, N.W. 19, S. 19, N.E. 21 S. 21, 22, 23, W. 24 S.W. 25, 27, S.E. 30, 31 N.W. 32, S.E. 32, 33, N. 34 S.E. 34, N. 35, S.W. 35, N.E. 36	53	8	5	(c) Hargwen and (c) Isfield See 22, 53, 8	Edmonton, Alta.
E. 1, 2, 3, 4 5, N. 6, S.E. 6, 7 8, 9, 12, 13 N.W. 14, 15, S. 16, 17 and 18 to 36, N.E. 16	54	8	5	(c) Isfield	do
Entire except Sec. 11, 29	55	8	5	do	do
1, 2, 3, N. 4 S.E. 4, 5, N. 6, S.E. 6 S. 7, 9, S.W. 10, 12 13, 15, N. 17, S. 21 N.W. 21, 25, 27, N.E. 28 S.W. 28, N. 30, S.W. 31*, 33 N.E. 34, N. 35, S.E. 35, N.W. 36	53	9	5	(c) Jarrow, Sec. 26, 53, 9	do
1, 2, N.E. 3, S. 3 W. 4, 7, 8, S. 9 N.W. 9, 12, 13, 14 17, E. 18, 19, S.E. 20 S.W. 21, 23, S. 24, N.W. 24 25, S.W. 26, 27, S.E. 19 N.W. 28, 31, N. 32, 33 N.W. 34, 35, 36	54	9	5	do	do
Entire except Sec. 11, 29	55	9	5	do	do
Entire except Sec. 11, 29, N.E. 34	56	9	5	(c) Jarrow	do
1, N.E. 2, S. 2, 3 4, 5, 6, 7 S.E. 8, 9, 10, 13 S.W. 14, 15, N.W. 16, 17 E. 18, N.W. 18, 19, S.W. 20 N.W. 22, 21, N.E. 24, 25 N.E. 26, S. 26, 27, N. 30 S.E. 30, N. 31, S.W. 31, N. 32 33, S. 34, 35, S. 36	53	10	5	(c) Keston, Sec. 25, 53, 10	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
Entire except Sec. 11, 29, S.W. 2.... N.E. 12, 24, 28, N.W. 34.....	54	10	5	(c) Leaman, Sec. 36, 53, 11	Edmonton, Alta.
Entire except Sec. 11, 29.....	55	10	5	do	do
Entire except Sec. 11, 29.....	56	10	5	do	do
Entire except Sec. 11, 29, N.E. 22, 25 N. 23, N.W. 24, S. 25, S. 26, S.E. 27. N.W. 35.....	53	11	5	(c) Leaman and (c) Mackay Sec. 8, 54, 11.....	do
.....2,5,631,32,3334,35,36.....	54	11	5	do	do
Entire except Sec. 11, 29.....	55	11	5	do	do
Entire except Sec. 11, 29.....	56	11	5	do	do
Entire except Sec. 11, 29.....	53	12	5	(c) Mackay and (c) Niton, Sec. 7, 54, 12.....	do
Entire except Sec. 11, N.E. 14..... N.E. 29.....	54	12	5	do	do
Entire except Sec. 11, 29.....	55	12	5	do	do
Entire except Sec. 11, 29.....	56	12	5	(c) Mackay and (c) Niton ..	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
Entire except N.E. 4, 20, 28, 32, N. 8. N.W. 9, 18, 32, 34 W. 10, 28, Secs. 11, 16, 29, 30, N.W. 18 S.E. 18, S. 32, E. 33, S.W. 33 N.W. 34	53	13	5	(c) Otley, Sec. 10, 54, 13.	Edmonton, Alta
Entire except Sec. 11, 29, N.W. 8. S.W. 5.	54	13	5	do	do
Entire except Sec. 11, 29	55	13	5	do	do
Entire except 11, 29.	56	13	5	do	do
Entire except 11, 29, N. 36, S.E. 36. N.W. 24, N.W. 35, S.E. 26.	53	14	5	(c) Peers, Sec. 9, 54, 14.	do
1, 2, 3, 4 5, 6, 7, 8 9, 10, 11, 12 13, 14, 15, 16 17, 18, 20, 21 22, 23, 24, 25	54	14	5	do	do
Entire except Sec. 11, 29	55	14	5	do	do
Entire except Sec. 11, 29.	56	14	5	do	do
Entire except Sec. 11, 29.	52	15	5	(c) Rosevear, Sec. 16, 54, 15.	do
Entire except Sec. 11, 29.	53	15	5	do	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
Entire except Sec. 11, 29, N.W. 17... N.E. 18, S.E. 19, N. 20, S.W. 20... N.W. 21, S.E. 21, N.E. 21... Portion of Sections 14, 15, 21, 22, 23, 24, 25, 26, 27, 28, 33, 34, 35, 36 in tim- ber berth 1095.....	54	15	5	(c) Rosevear, Sec. 16, 54, 15	Edmonton, Alta.
Entire except Sec. 11, 29.....	55	15	5	(c) Rosevear.....	do
Except entire Sec. 11, 29.....	52	16	5	(c) Thornton, Sec. 2, 54, 16	do
Entire except Sec. 11, 29, N.W. 27... N.E. 28, N. 34, N.W. 35, N. 36, ... S.W. 36, S. ½ S.W. 33*.....	53	16	5	(c) Thornton and (c) Yate Sec. 32, 53, 16.....	do
Entire except N. 1 S.E. 1, S.W. 1... S. 3, N.W. 3, N.E. 4, S.W. 4, N.W. 12 S. 12, S.W. 14, N.E. 15, S.W. 15 Sec. 11, 29..... Portion covered by timber berth 1095.....	54	16	5	do	do
Entire except Sec. 11, 29.....	52	17	5	(c) Edson, Sec. 15, 53, 17....	do
Entire except Sec. 11, 15, 16, 22, 29... N.W. 2, 3, 4, 10, S.W. 2, 9, 17..... N.E. 3, 4, 9, 10, 21, S.E. 9, 17, 20, S. 10 E. 12, W. 14, 21, 23, N. 17..... S.E. 21, S.E. 26, N.W. 28..... N.E. 26, S.E. 24, S.W. 24..... N.W. 13.....	53	17	5	do	do
Entire except 11, 29, S.E. 36..... Portion covered by timber berths 1092, 1343.....	52	18	5	Station not located.....	do
Entire except Sec. 11, 29, S.W. 10, 21 N.W. 2, 3, S.E. 20.....	53	18	5	do	do

Canadian Government Lands—Continued

SECTION	T'shp.	R'ge	West. Mer.	NEAREST STATION (Location)	DOMINION LANDS OFFICE
Entire except 11, 29.....	52	19	5	Station not located.....	Edmonton, Alta.
Portion covered by timber berths 1093, 1343.....					
Entire except Sec. 11, 29, N.W. 5, 6, S.E. 6, 7, S.W. 5, S.E. 13.....	53	19	5	do.....	do
.....					
Entire except Sec. 11, 29.....	52	20	5	do.....	do
Portions covered by timber berths 1093.....					
All West clear.....	52	20	5	do.....	do
S.W. 30, S.E. 30.....					

NEW TOWNS ALONG THE GRAND TRUNK PACIFIC RAILWAY

The Railway Company has selected and permanently located eighty townsites in the most favorable situations on the main line west of Winnipeg.

During the construction of the railway the divisional points townsites have naturally shown the greatest growth, but there are many other townsites so located that they have become important places; developing very rapidly, prominent among which are:

Lazare, Man	Raymore, Sask.	Unity, Sask.
Spy Hill, Sask.	Semans "	Irma, Alta.
Atwater "	Nokomis "	Meighen "
Waldron "	Kinley "	Bruce "
Fenwood "	Landis "	Holden "
Kelliher "	Coblenz "	Ryley "
Punnichy "	Scott "	Tofield "

The Divisional townsites are:

Rivers, Man., pop. 650 when six months old. Had two lumber yards, thirty business buildings and fifty-four dwellings.

Melville, Sask., pop. 700 when six months old, 1,400 when less than a year old. Had three lumber yards and 350 buildings.

Watrous, Sask., pop. 300 when four months old. Had five lumber yards and forty-five buildings.

Wainwright, Alta., pop. 400 when four months old. Had two lumber yards and eighty buildings.

Chamberlin, Alta., opened in 1909.

Attention is particularly called to the fact that the townsite lots are being offered at prices which are in keeping with the present conditions, and it is natural to infer that the value of the lots in these main line distributing centres will increase with the development of the surrounding country.

Prices for lots at Divisional Points range from \$50.00 to \$400.00, at other townsites from \$50.00 to \$250.00; terms one-third cash, balance in six and twelve months, with interest at eight per cent. per annum. Lots are twenty-five or fifty feet frontage and 140 feet deep. A purchaser may obtain ten or more lots which may be termed not first-class, at a reduction of ten per cent, to twenty per cent. off the usual prices for the lots. Further information can be obtained and plans procured upon applying to:

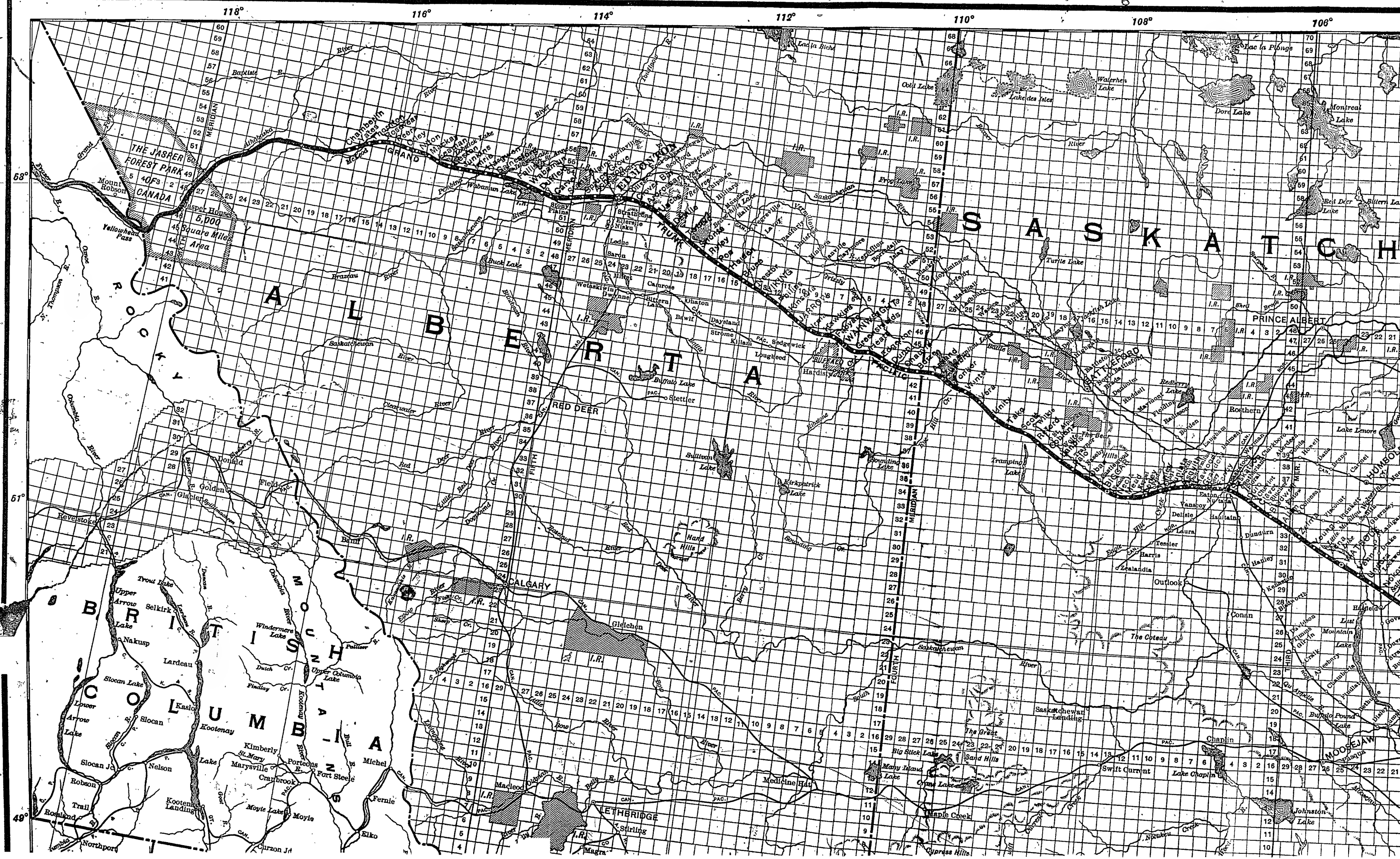
G. U. RYLEY,

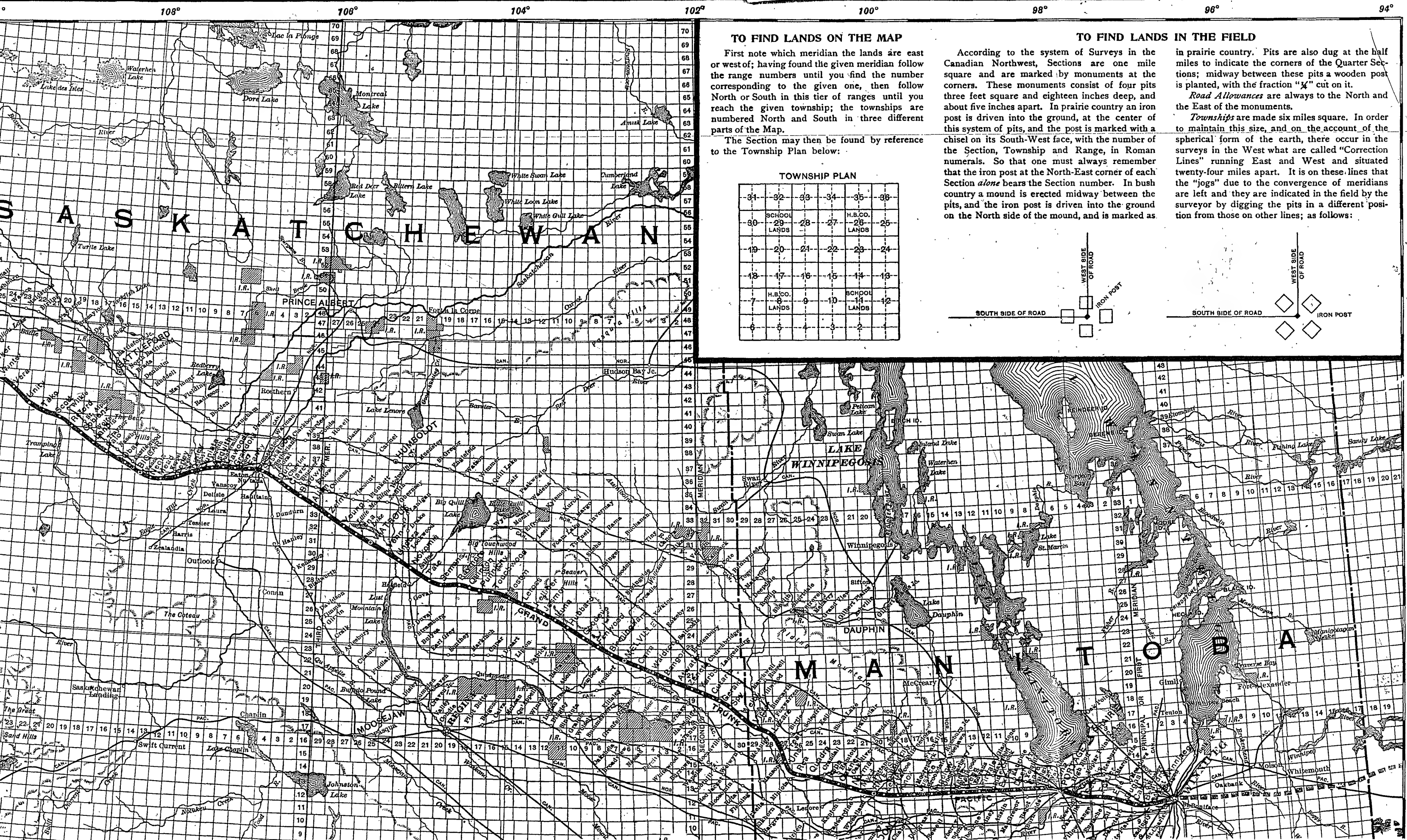
Land Commissioner Grand Trunk Pacific Ry.

WINNIPEG, MAN.

PAMPHLET L 2 pertaining to lands
along the Grand Trunk Pacific
may be obtained from the General
Passenger Agent at Winnipeg, or Divisional
or Principal Passenger and Ticket Agents
of the Grand Trunk Pacific and Grand
Trunk Railway Systems upon application.

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TO FIND LANDS ON THE MAP

First note which meridian the lands are east or west of; having found the given meridian follow the range numbers until you find the number corresponding to the given one, then follow North or South in this tier of ranges until you reach the given township; the townships are numbered North and South in three different parts of the Map.

The Section may then be found by reference to the Township Plan below:

TOWNSHIP PLAN

31	32	33	34	35	36
30	SCHOOL LANDS	28	27	H.B.CO. LANDS	25
19	20	21	22	23	24
18	17	16	15	14	13
7	H.B.CO. LANDS	9	10	SCHOOL LANDS	12
6	5	4	3	2	1

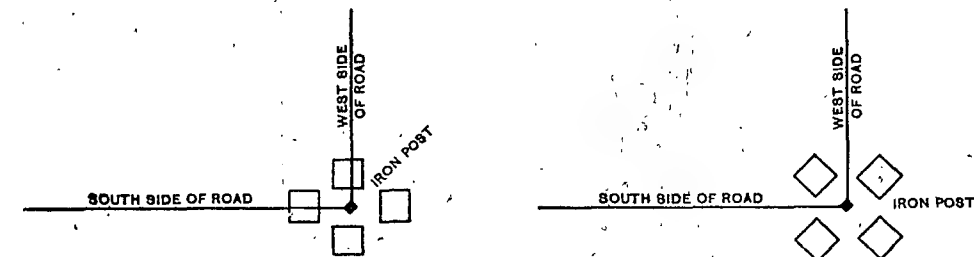
TO FIND LANDS IN THE FIELD

According to the system of Surveys in the Canadian Northwest, Sections are one mile square and are marked by monuments at the corners. These monuments consist of four pits three feet square and eighteen inches deep, and about five inches apart. In prairie country an iron post is driven into the ground, at the center of this system of pits, and the post is marked with a chisel on its South-West face, with the number of the Section, Township and Range, in Roman numerals. So that one must always remember that the iron post at the North-East corner of each Section alone bears the Section number. In bush country a mound is erected midway between the pits, and the iron post is driven into the ground on the North side of the mound, and is marked as

in prairie country. Pits are also dug at the half miles to indicate the corners of the Quarter Sections; midway between these pits a wooden post is planted, with the fraction "1/4" cut on it.

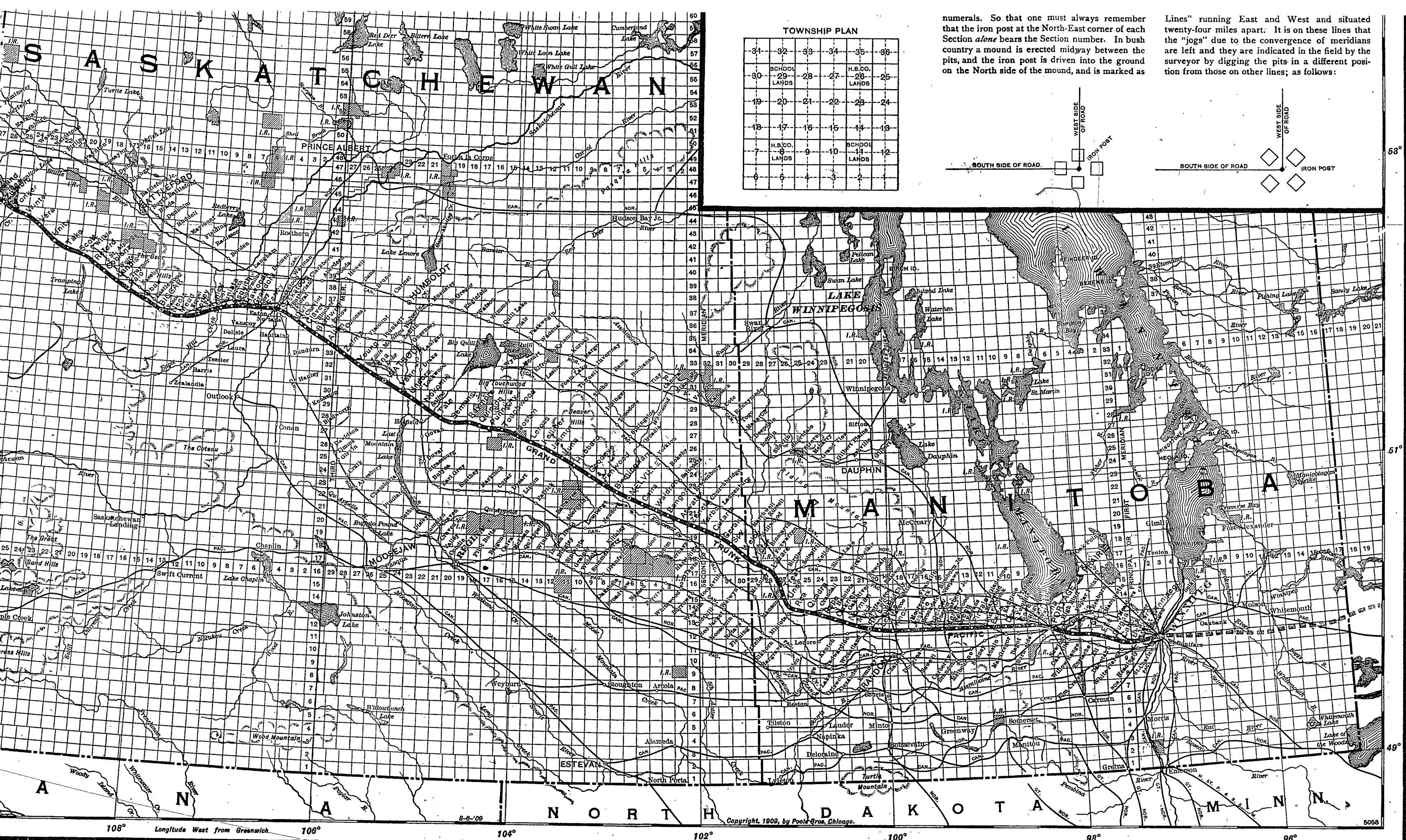
Road Allowances are always to the North and the East of the monuments.

Townships are made six miles square. In order to maintain this size, and on the account of the spherical form of the earth, there occur in the surveys in the West what are called "Correction Lines" running East and West and situated twenty-four miles apart. It is on these lines that the "jogs" due to the convergence of meridians are left and they are indicated in the field by the surveyor by digging the pits in a different position from those on other lines; as follows:



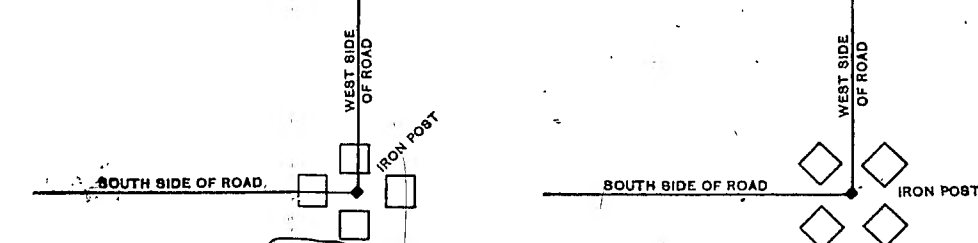


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TOWNSHIP PLAN

34	32	33	34	35	36
30	29	28	27	26	25
19	20	21	22	23	24
13	17	16	15	14	13
7	8	9	10	11	12
6	5	4	3	2	1



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ANNOUNCEMENT

IN response to many inquiries on the subject which have been received by the Company from all parts of the world, a booklet has been issued by the Grand Trunk Pacific Railway Company for the purpose of giving information respecting its western terminus on the Pacific Ocean at

PRINCE RUPERT BRITISH COLUMBIA

the new city now being built on the northern British Columbia Coast.

There has been acquired in the interest of the Railway Company twenty-four thousand acres of land at Prince Rupert and vicinity for the purpose of the townsite and the development of the Port, one quarter interest in which belongs to the *Province of British Columbia*, who are therefore jointly interested with the Railway Company in the development of this *New Seaport*.

The first subdivision of the townsite will cover an area of about two thousand acres and

WILL BE OPENED TO THE PUBLIC AND SOLD ON OR ABOUT MAY 1st, 1909

The manner in which the lots will be offered for sale, the places at which they will be sold and the prices and terms of payment will be given wide publicity sufficiently in advance to enable every one desiring to purchase lots to make arrangements to do so.

In the meantime permission cannot be given to start in business or erect buildings until plans for the opening are completed.

On account of misleading announcements which have been published by land speculators in the vicinity of Prince Rupert where the name has been used without authority, care should be taken to see that any announcement in regard to this new seaport is officially made by the Grand Trunk Pacific Railway Company.

Copies of this booklet may be obtained upon application to the Secretary of the Grand Trunk Pacific Railway Company, Montreal, the Land Commissioner at Winnipeg, Manitoba, Canada, or the General Passenger Agent at Winnipeg, Manitoba.



To



